



AGENDA

LITTLE CHUTE VILLAGE BOARD COMMITTEE OF THE WHOLE MEETING

PLACE: Little Chute Village Hall
DATE: Wednesday, May 11, 2022
TIME: 6:00 p.m.

Virtually attend the May 11, Committee of the Whole meeting at 6 PM by following the zoom link here:

Join Zoom Meeting

<https://us06web.zoom.us/j/83651057664>

Meeting ID: 836 5105 7664

Dial by your location: +1 312 626 6799 US (Chicago)

REGULAR ORDER OF BUSINESS

- A. Invocation
- B. Roll call of Trustees
- C. Roll call of Officers and Department Heads
- D. Public Appearance for Items Not on the Agenda

- E. Discussion—Pine Street Plan

- F. Call for Unfinished Business

- G. Items for Future Agenda

- H. Adjournment

Requests from persons with disabilities who need assistance to participate in this meeting or hearing should be made with as much advance notice as possible to the Clerk's Office at 108 West Main Street, (920) 423-3852, [email: Laurie@littlechutewi.org](mailto:Laurie@littlechutewi.org) Prepared: May 6, 2022



Item For Consideration

For Board Review On: May 4 2022

Agenda Item Topic: Pine Street Plan Discussion

Prepared On: May 6 2022

Prepared By: Admin. Bernhoft

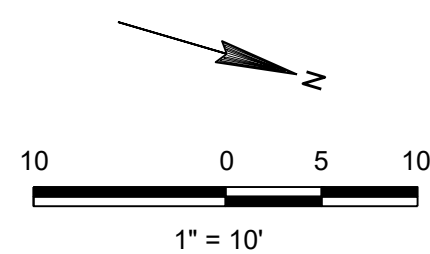
Report: The plans below are the updates requested by the Village Board for the Pine Street pedestrian mall project for your review. These changes check the primary boxes of the project needs such as utilities, closing of Pine Street for vehicle traffic and curb cleanup to Main Street and the south end of the property. Due to the changes depicted in the plans, it is important to give the public and local business owners an opportunity to review and provide feedback on the scope of work. With the work to be completed as presented, we will need to plan future renovations to the Village property such as pavement and access through the CIP process.

Fiscal Impact: Cost estimates for this project come in at \$321,743.88 (see below for breakdown). The current budget on Pine Street is \$1,190,072 (TID 8 Project), but those costs covered a full reconstruction of the existing parking lot with stormwater management.

Recommendation/Board Action: No action is needed at this meeting. This is an opportunity for the public stakeholders to review and provide comments on the project as presented. The Board can direct staff to make changes or continue as presented.

Respectfully Submitted,

Beau Bernhoft, Village Administrator



REV	DATE	BY	DESCRIPTION

2022 PINE STREET RECONSTRUCTION
PINE STREET
VILLAGE OF LITTLE CHUTE
OUTAGAMIE COUNTY, WISCONSIN



VILLAGE OF LITTLE CHUTE STAFF

Village Engineer:

Christopher L. Murawski, P.E.
Engineering Department
108 West Main Street
Little Chute, Wisconsin 54140

chris@littlechutewi.org

Office Phone: (920) 423-3861
Cell Phone: (920) 841-0042

Engineering Technician:

Rob Olkiewicz
Engineering Department
108 West Main Street
Little Chute, Wisconsin 54140

rob@littlechutewi.org

Office Phone: (920) 423-3866
Cell Phone: (920) 422-1947

Engineering Technician:

Bryan Blum
Engineering Department
108 West Main Street
Little Chute, Wisconsin 54140

bryan@littlechutewi.org

Office Phone: (920) 423-3862
Cell Phone: (920) 422-0938

Engineering Technician:

Kris Lyons, CPESC
Engineering Department
108 West Main Street
Little Chute, Wisconsin 54140

kris@littlechutewi.org

Office Phone: (920) 423-3864
Cell Phone: (920) 422-4840

Engineering Technician:

Dave Mattson
Engineering Department
108 West Main Street
Little Chute, Wisconsin 54140

david@littlechutewi.org

Office Phone: (920) 423-3863
Cell Phone: (920) 378-4395

DRAWING INDEX

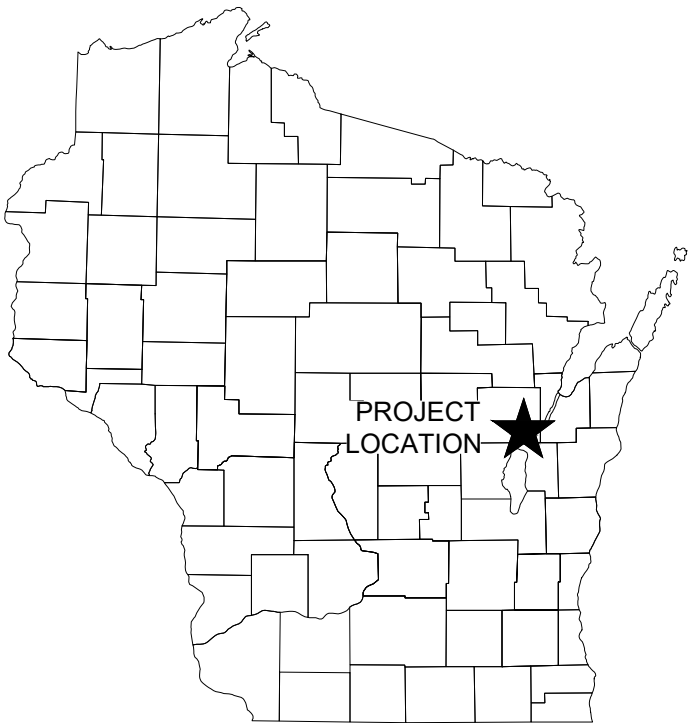
REFERENCE NO.	DESCRIPTION
0.0	COVER SHEET
1.0	ABBREVIATIONS, LEGEND & NOTES
1.1	SURVEY CONTROL & PROPERTY OWNER INFO
2.0 - 2.1	UTILITIES PLAN SHEETS
3.0 - 3.2	GRADING & PAVING PLAN SHEETS
4.0	DEMOLITION PLAN SHEETS
5.0	SURFACE RESTORATION PLAN SHEETS
6.0	EROSION CONTROL PLAN SHEET
7.0	PARKING LOT STRIPING PLAN SHEET
8.0 - 8.9	MISCELLANEOUS CONSTRUCTION DETAILS
9.0 - 9.1	QUANTITIES
10.0	STORM SEWER MODELING

ISSUED

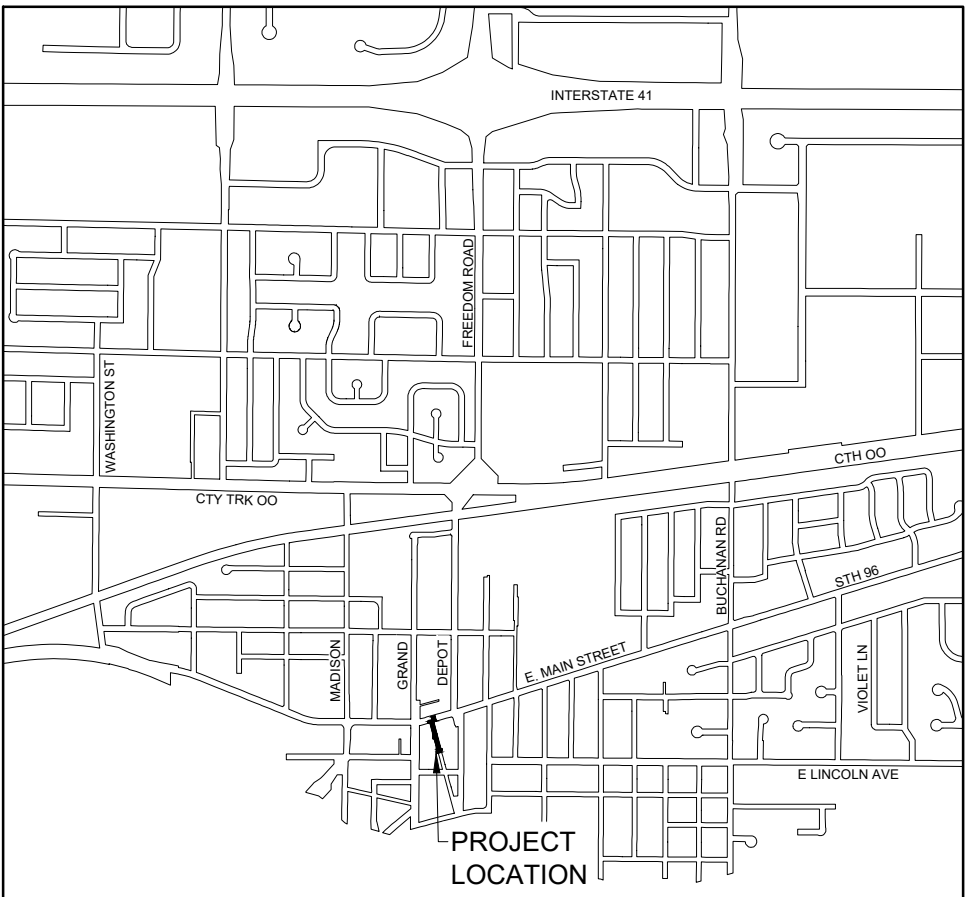
DATE	DESCRIPTION
XX/XX/2022	ISSUED FOR BID

PROJECT NUMBER: 2022003

0.0



NOT TO SCALE



1000 500 0 1000

1" = 40'

UTILITY CONTACT INFORMATION

AT&T MIDWEST (TELEPHONE)

KYLE WEBER
205 S. JEFFERSON STREET
GREEN BAY, WI 54301
920-221-5969
KW715W@ATT.COM

CHARTER (CABLE)

VINCE ALBIN
3520 DESTINATION DRIVE
APPLETON, WI 54915
920-378-0444
VINCE.ALBIN@CHARTER.COM

KAUKAUNA UTILITIES (ELECTRIC)

BRIAN WILLMS
777 ISLAND STREET
KAUKAUNA, WI 54130
920-462-0222
BWILLMS@KU-WI.ORG

WE ENERGIES (GAS)

HEATHER DEUTH
800 S. LYNNDAL DRIVE
APPLETON, WI 54912
Office: 920-380-3464
Cell: 920-242-5633
HEATHER.DEUTH@WE-ENERGIES.COM

WE ENERGIES (ELECTRIC)

ZACH DUGA
800 S. LYNNDAL DRIVE
APPLETON, WI 54912
Office: 920-380-3458
Cell: 920-450-9314
ZACHARY.DUGA@WE-ENERGIES.COM

FOX VALLEY METRO POLICE

200 W. MCKINLEY AVE
LITTLE CHUTE, WI 54140
NON EMERGENCY
920-788-7505

LITTLE CHUTE FIRE DEPARTMENT

200 W. MCKINLEY AVE
LITTLE CHUTE, WI 54140
NON EMERGENCY
920-788-7399

MIDWEST CONTRACT OPERATIONS (WATER)








































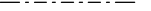
































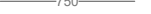




















































JERRY VERSTEGEN
625 E. EVERGREEN DRIVE
LITTLE CHUTE, WI 54140
920-788-7522



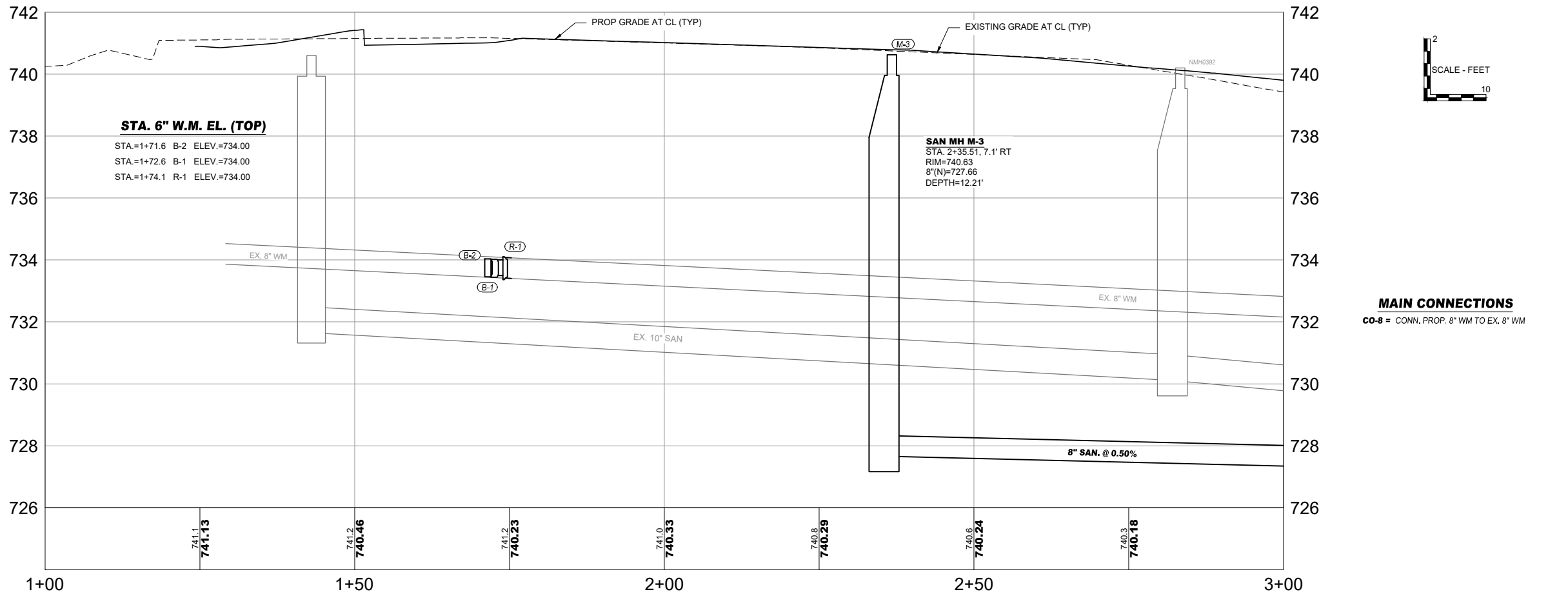
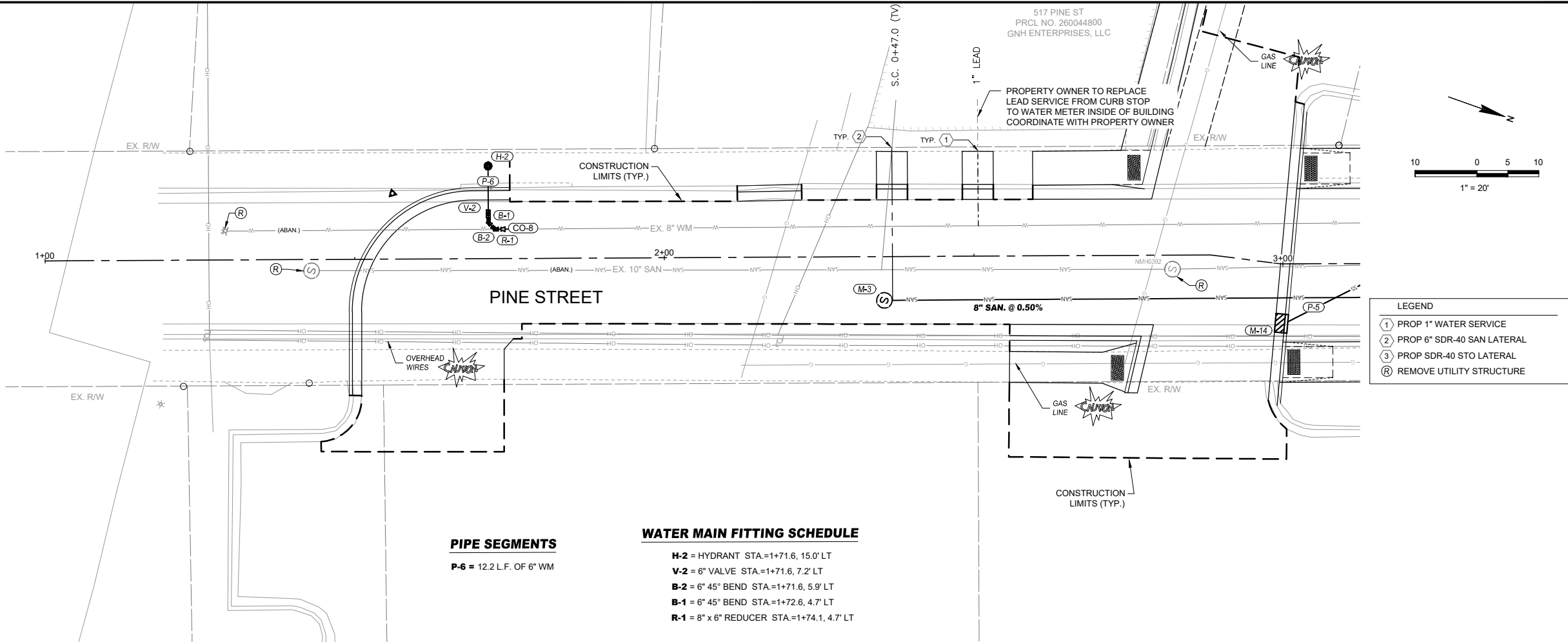
Dial 811 or (800)242-8511

www.DiggersHotline.com

Q:\Dept of Public Works\Engineering Department\2000 - Municipal Projects\Pine St. - Main - Vacation\10 - DWG\SHEETS\0.0 to 1.1 Pine Cover Sheets_Grass.dwg, 5/22/2022 2:33:25 PM, KRIS LYONS, _Little Chute-CSD-2018.cb

EXISTING SYMBOLS		EXISTING FEATURES LEGEND		EROSION CONTROL LEGEND		PROPOSED SYMBOLS		PROPOSED FEATURES LEGEND		STANDARD ABBREVIATIONS			
	BENCHMARK		CENTERLINE OR REFERENCE LINE		EROSION MAT CLASS I, TYPE A OR B		STORM MANHOLE		CENTERLINE OR REFERENCE LINE	ABAN	ABANDON	MAINT	MAINTENANCE
	CONTROL POINT		SANITARY SEWER MAIN		EROSION MAT CLASS II OR III (EXCEPT II, TYPE A)		OPEN GRATE INLET		SANITARY SEWER MAIN	AC	ACRE	MATL	MATERIAL
	MAILBOX		SANITARY SEWER LATERAL		RESTORATION W/ HYDROMULCH		2'x3' INLET TYPE L GRATE		SANITARY SEWER LATERAL	AEW	APRON ENDWALL	MAX	MAXIMUM
	STORM MANHOLE		STORM SEWER MAIN		SILT FENCE		APRON ENDWALL		STORM SEWER MAIN	ASPH	ASPHALT PAVEMENT	MH	MANHOLE
	SANITARY MANHOLE		STORM SEWER LATERAL		TURBIDITY BARRIER		SANITARY MANHOLE		STORM SEWER LATERAL	AVG	AVERAGE	NB	NORTHBOUND
	ELECTRIC MANHOLE		STORM SEWER MINI SEWER		TRACKOUT DEVICE		CLEAN OUT		WATER MAIN	BB	BURY BOLT	NO	NUMBER
	WATER VALVE		WATER MAIN		SURFACE WATER FLOW		WATER VALVE		WATER SERVICE	BC-BC	BACK CURB TO BACK CURB	NOR	NORMAL
	HYDRANT		WATER SERVICE		EROSION BALE, BARRIER		HYDRANT		SLOPE INTERCEPT	BEG	BEGIN	OD	OUTSIDE DIAMETER
	GAS VALVE		OVERHEAD UTILITY		TEMPORARY DITCH CHECK		WATER MANHOLE		CONSTRUCTION LIMITS	BIT	BITUMINOUS	PAV/T	PAVEMENT
	ELECTRICAL BOX or PEDESTAL		UNDERGROUND TELEPHONE		RIPRAP		CURB STOP		CURB AND GUTTER	BK	BACK	PC	POINT OF CURVATURE
	TELEPHONE BOX or PEDESTAL		UNDERGROUND ELECTRIC		EXISTING INLET INLET PROTECTION (TYPE D-HR)		CLEAN OUT		INTEGRAL CURB AND GUTTER	B/L	BASE LINE	PCC	PORTLAND CEMENT CONCRETE
	FIBER OPTIC PEDESTAL		UNDERGROUND FIBER OPTIC		PROPOSED INLET INLET PROTECTION (TYPE D-HR)		WATER MAIN TEE		FENCE	BLDG	BUILDING	PE	PRIVATE ENTRANCE
	POWER POLE		UNDERGROUND GAS LINE		CULVERT PIPE CHECK		WATER VALVE		MAJOR CONTOUR	CY	CUBIC YARD	PED	PEDESTAL
	LIGHT POLE		UNDERGROUND COMMUNICATION				WATER MAIN CROSS		MINOR CONTOUR	C&G	CURB AND GUTTER	PGL	PROFILE GRADE LINE
	SIGN		UNDERGROUND TELEVISION				WATER MAIN 90° BEND		EASEMENT	CB	CATCH BASIN	PI	POINT OF INTERSECTION
	WATER MANHOLE		PAVEMENT EDGE				WATER MAIN 45° BEND			CE	COMMERCIAL ENTRANCE	PIL	PROPERTY LINE
	TELEPHONE MANHOLE		GRAVEL EDGE				WATER MAIN 22.5° BEND			CHD	CHORD	PP	POWER POLE
	GUY WIRE		DITCH CENTERLINE				WATER MAIN REDUCER			CIL	CENTER LINE	PRC	POINT OF REVERSE CURVATURE
	CLEAN OUT		BUILDING LINE				CAP AND PLUG (UTILITY)			CL	CLASS OF PIPE	PROPOSED	PROPOSED
	CONIFEROUS TREE or BUSH		CHAINLINK FENCE				SIGN			CMP	CORRUGATED METAL PIPE	PSI	POUNDS PER SQUARE INCH
	DECIDUOUS TREE or BUSH		WOOD FENCE				ASPHALT			CO	CLEAN OUT	RD	ROAD
	STUMP		BRUSHLINE				CONCRETE SIDEWALK			CONC	CONCRETE	REBAR	REINFORCEMENT ROD
	CURB STOP		UTILITY EASEMENT				CONCRETE DRIVEWAY / RAMP			CORR	CORRUGATED	REM	REMOVE
	STORM SEWER CATCH BASIN or INLET		DELINEATED WETLANDS				DETECTABLE WARNING PLATE			CP	CONTROL POINT	RECON	RECONSTRUCT
	RIPRAP		CONCRETE CURB AND GUTTER				PROP 1 1/4" WATER SERVICE			CPPK	CONTROL POINT NAIL	REQ'D	REQUIRED
	SECTION CORNER		GUARDRAIL				PROP 6" SDR-40 SAN LATERAL			CR	CRUSHED	R/L	REFERENCE LINE
	IRON PIPE		WATER EDGE				PROP 6" SDR-40 STO LATERAL			CS	CURB STOP	RP	RADIUS POINT
	PK NAIL		SIDEWALK							CSW	CONCRETE SIDEWALK	RR	RAILROAD
	CULVERT END SECTION		MAJOR CONTOUR							CTH	COUNTY TRUNK HIGHWAY	RT	RIGHT
	LIGHT POLE W/ MAST ARM		MINOR CONTOUR							CULV	CULVERT	R/W	RIGHT-OF-WAY
	AIR CONDITIONER		DRIVEWAY							D	DEPTH OR DELTA	RW	RETAINING WALL
	BOLLARD		SLOPE TOP or BOTTOM							DI	DUCTILE IRON	SB	SOUTHBOUND
	FLAG POLE		TREELINE							DIA	DIAMETER	SE	SUPERELEVATION
	MONITORING WELL		RIGHT-OF-WAY							EA	EACH	SF	SQUARE FEET
	SPRINKLER HEAD		RAILROAD TRACKS										

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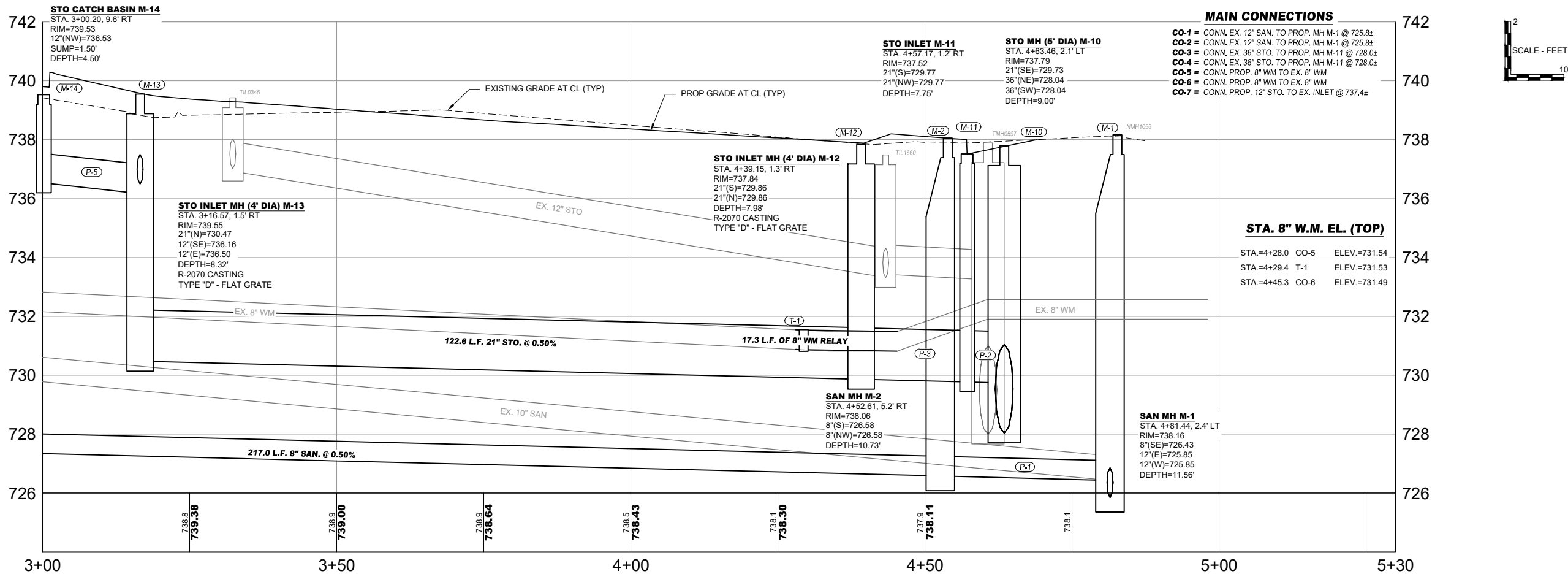
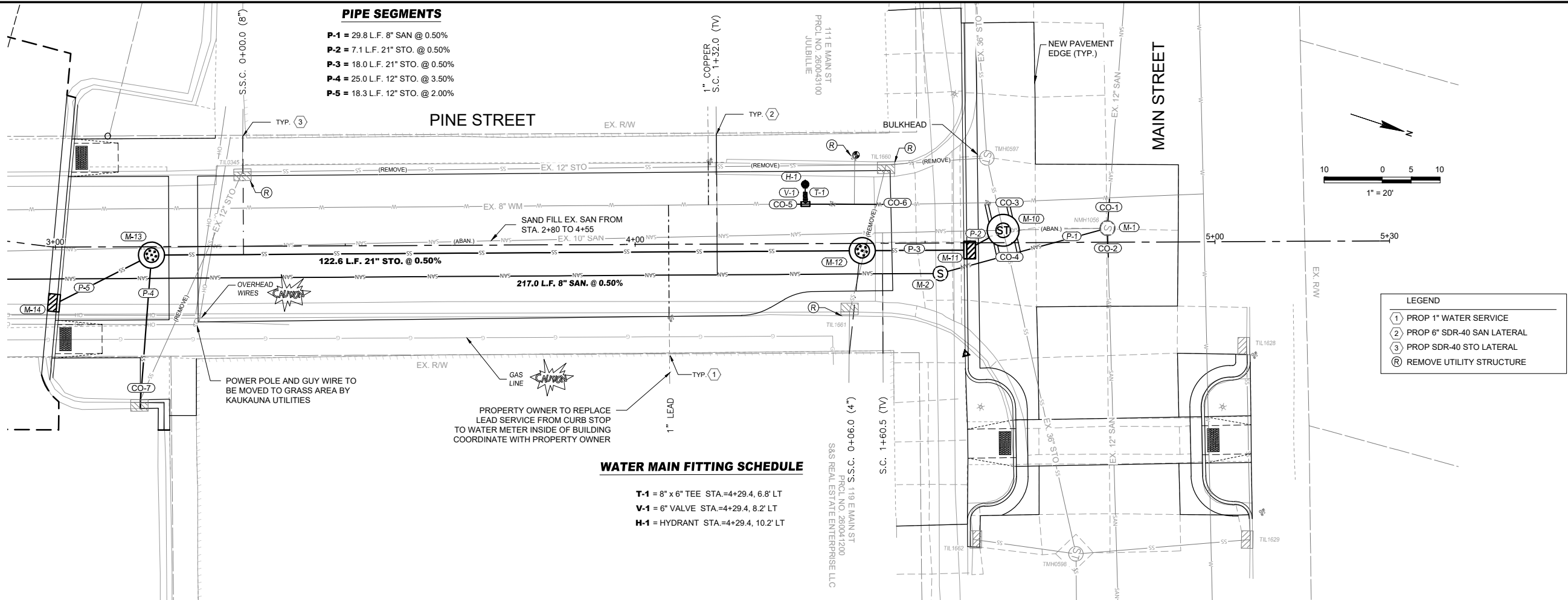


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CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
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SHEET REFERENCE NO.

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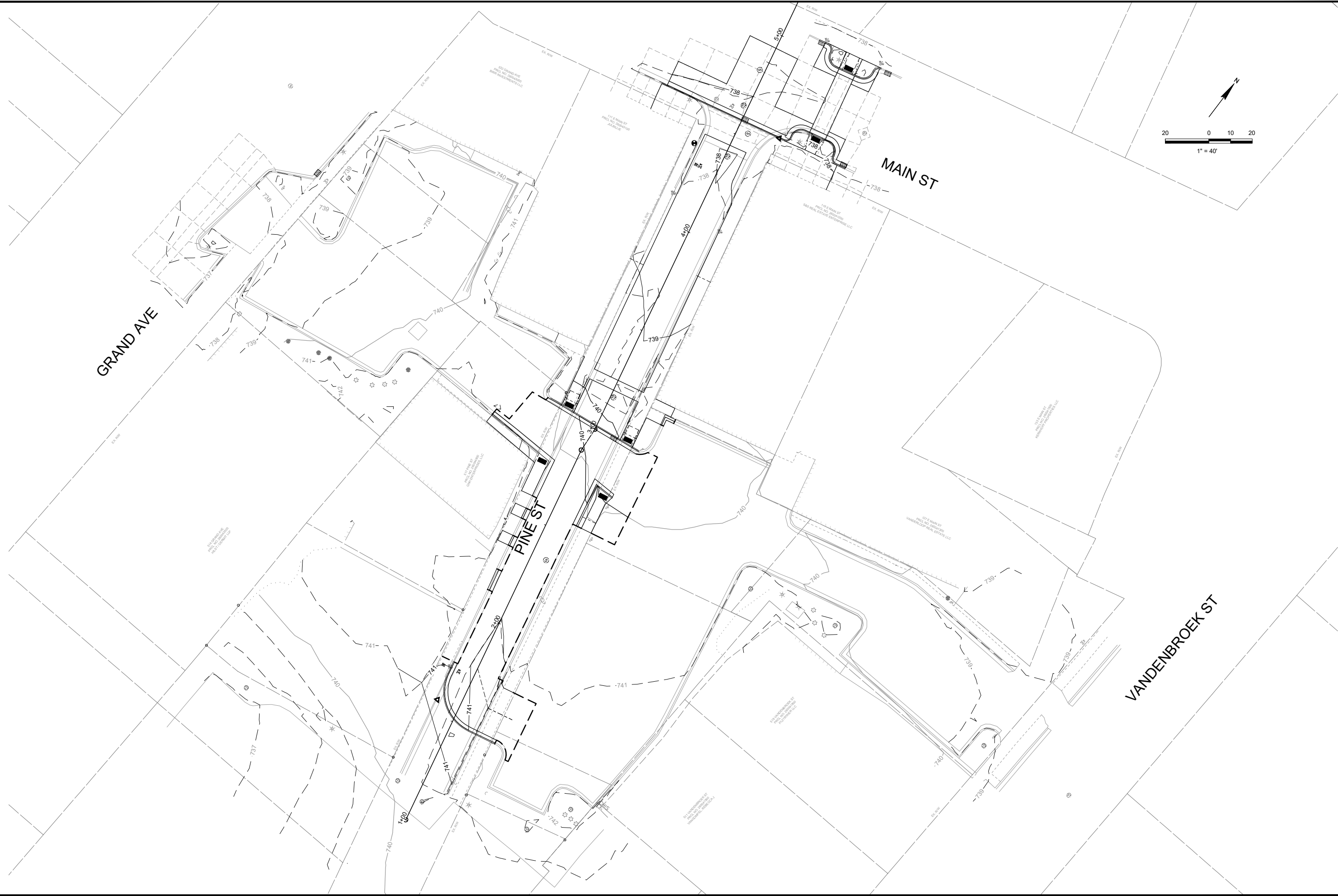
2022 PINE STREET RECONSTRUCTION
UTILITY PLAN FROM STA. 3+00 TO MAIN STREET
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

REV	DATE	BY	DESCRIPTION
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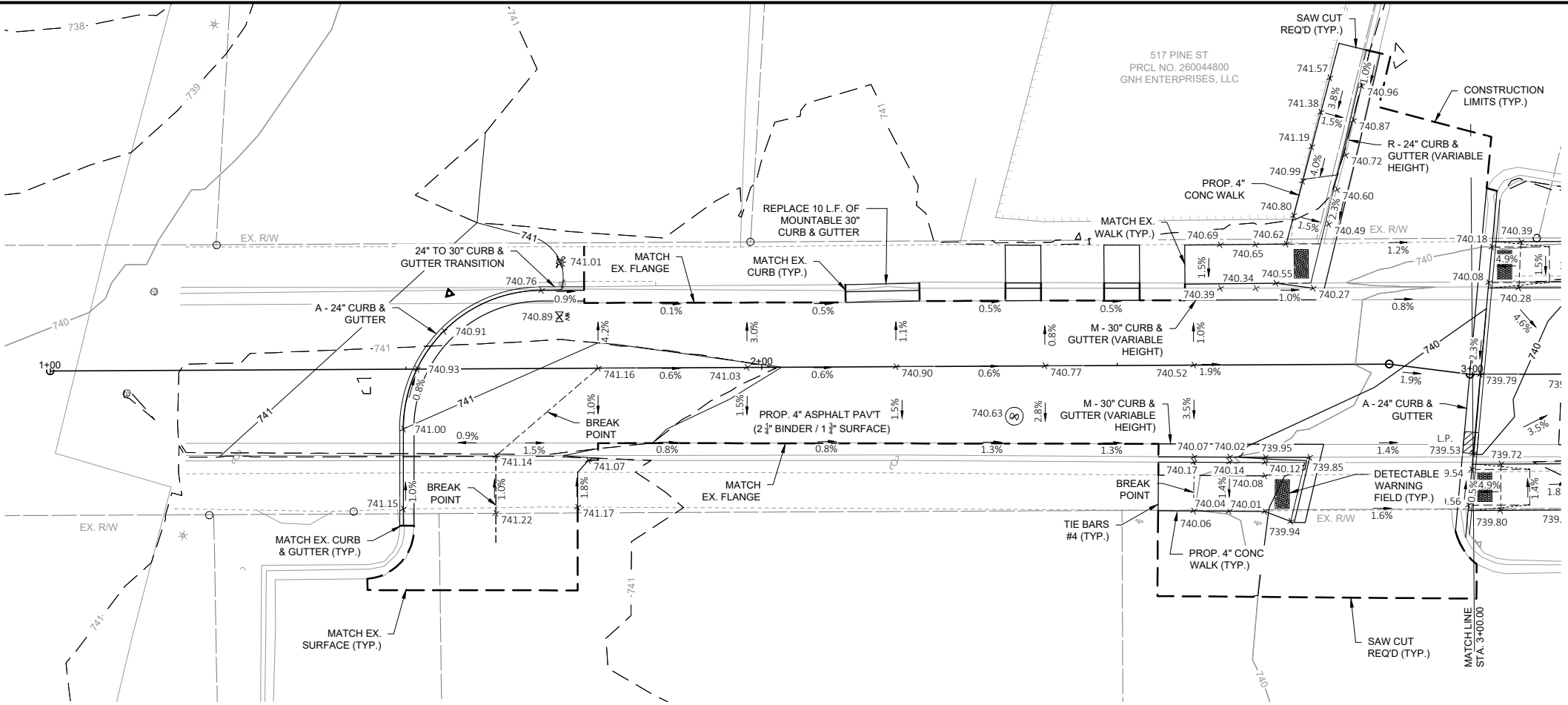
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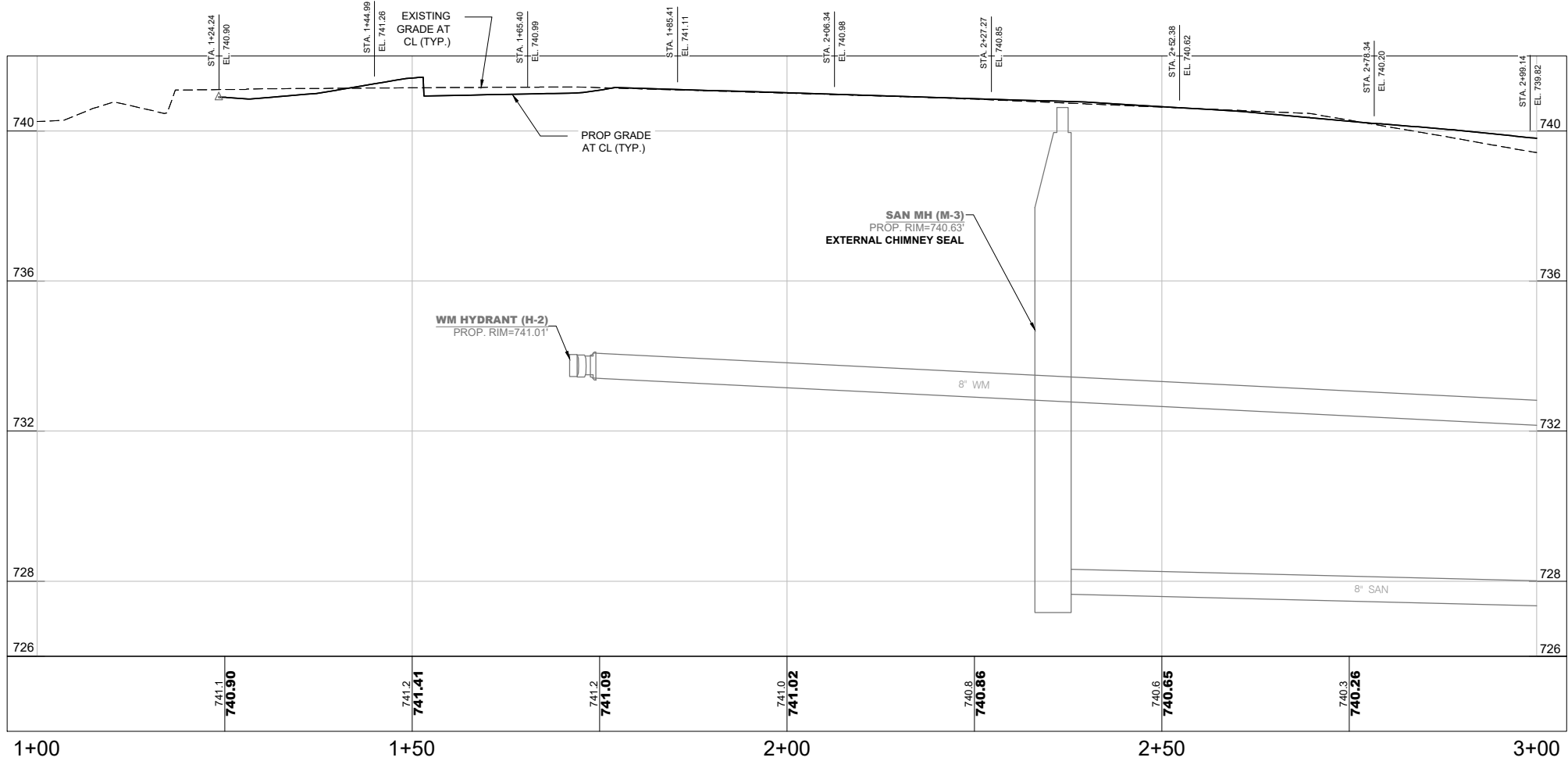
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2022 PINE STREET RECONSTRUCTION
OVERALL GRADING PLAN
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

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KEY
A = ACCEPTING SLOPE CURB & GUTTER
R = REVERSE SLOPE CURB & GUTTER
M = MOUNTABLE CURB & GUTTER



2022 PINE STREET RECONSTRUCTION
GRADING PLAN STA. 1+00 TO STA. 3+00
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

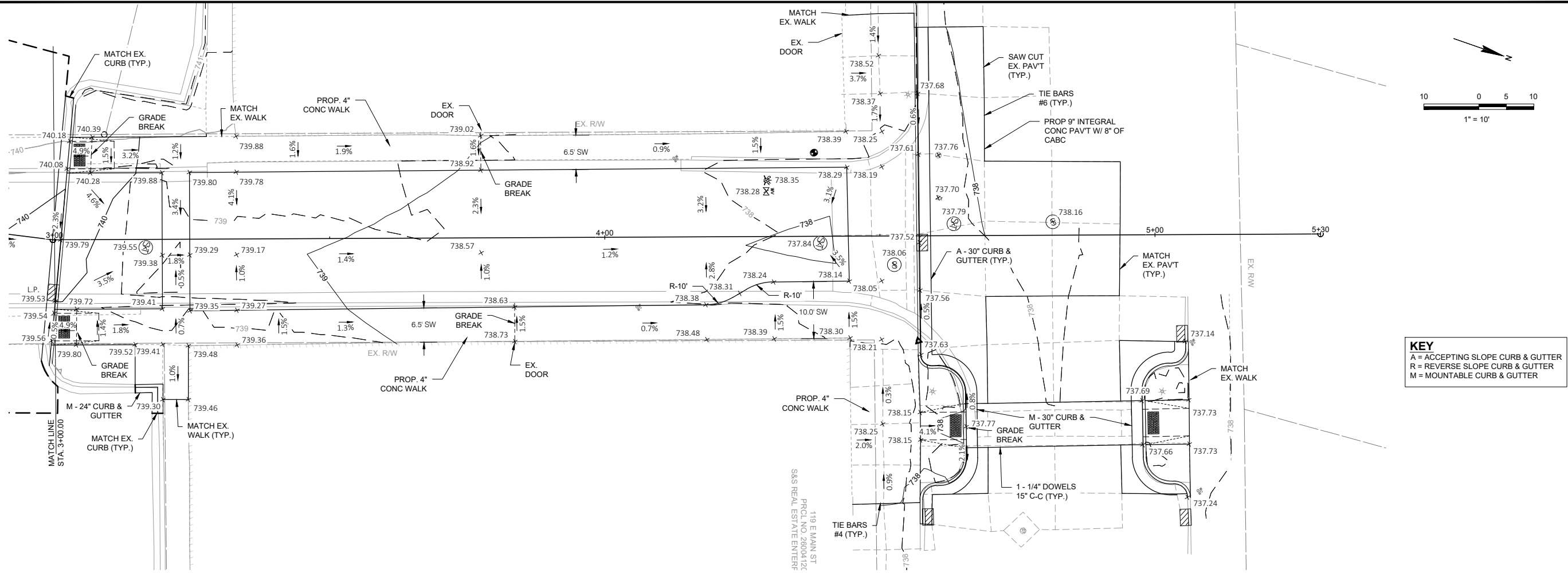
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APPROVED: CLM

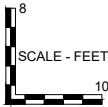
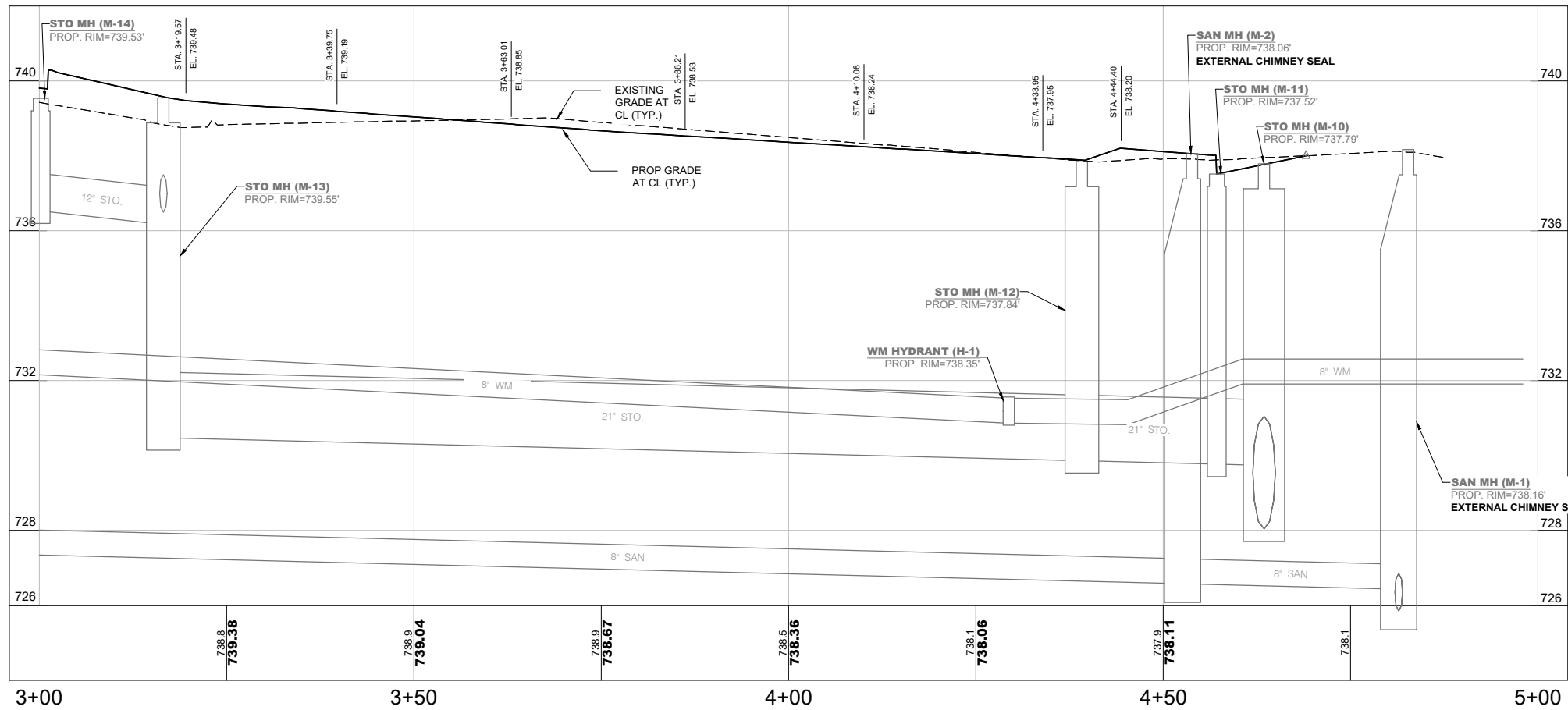
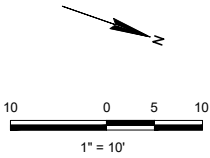
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2022003
SHEET REFERENCE NO.

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KEY
A = ACCEPTING SLOPE CURB & GUTTER
R = REVERSE SLOPE CURB & GUTTER
M = MOUNTABLE CURB & GUTTER



2022 PINE STREET RECONSTRUCTION
GRADING PLAN STA. 3+00 TO MAIN STREET
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

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PROJECT NUMBER
2022003
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3.2

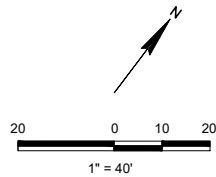
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GRAND AVE

PINE ST

MAIN ST

VANDEBROEK ST



LIMITS OF GRADING
DISTURBANCE
(TYP.)

REMOVE EX.
WALK (TYP.)

REMOVE EX.
GRAVEL (TYP.)

REMOVE CURB &
GUTTER (TYP.)

REMOVE CURB &
GUTTER (TYP.)

REMOVE EX.
WALK (TYP.)

ONE WAY
←
REMOVE

REMOVE EX.
WALK (TYP.)

REMOVE EX.
WALK (TYP.)

DO NOT
ENTER
REMOVE

ONE WAY
→
REMOVE

SAW CUT
EX. PAVT
(TYP.)

REMOVE PAVEMENT
MARKINGS FROM
HANDI-CAP AREA
(TYP.)

SALVAGE & REINSTALL
EX. CONC. BUMPERS (TYP.)

NO PARKING
8 AM - 6 PM
DEC 1st - APRIL 1st
P
REMOVE

REMOVE EX.
ASPHALT (TYP.)

DISTURBANCE
LIMITS (TYP.)

REMOVE EX.
ASPHALT (TYP.)

SAW CUT
EX. PAVT
(TYP.)

REMOVE CURB &
GUTTER (TYP.)

SALVAGE
EX. BIKE
RACK (BY
OTHERS)

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

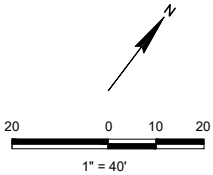
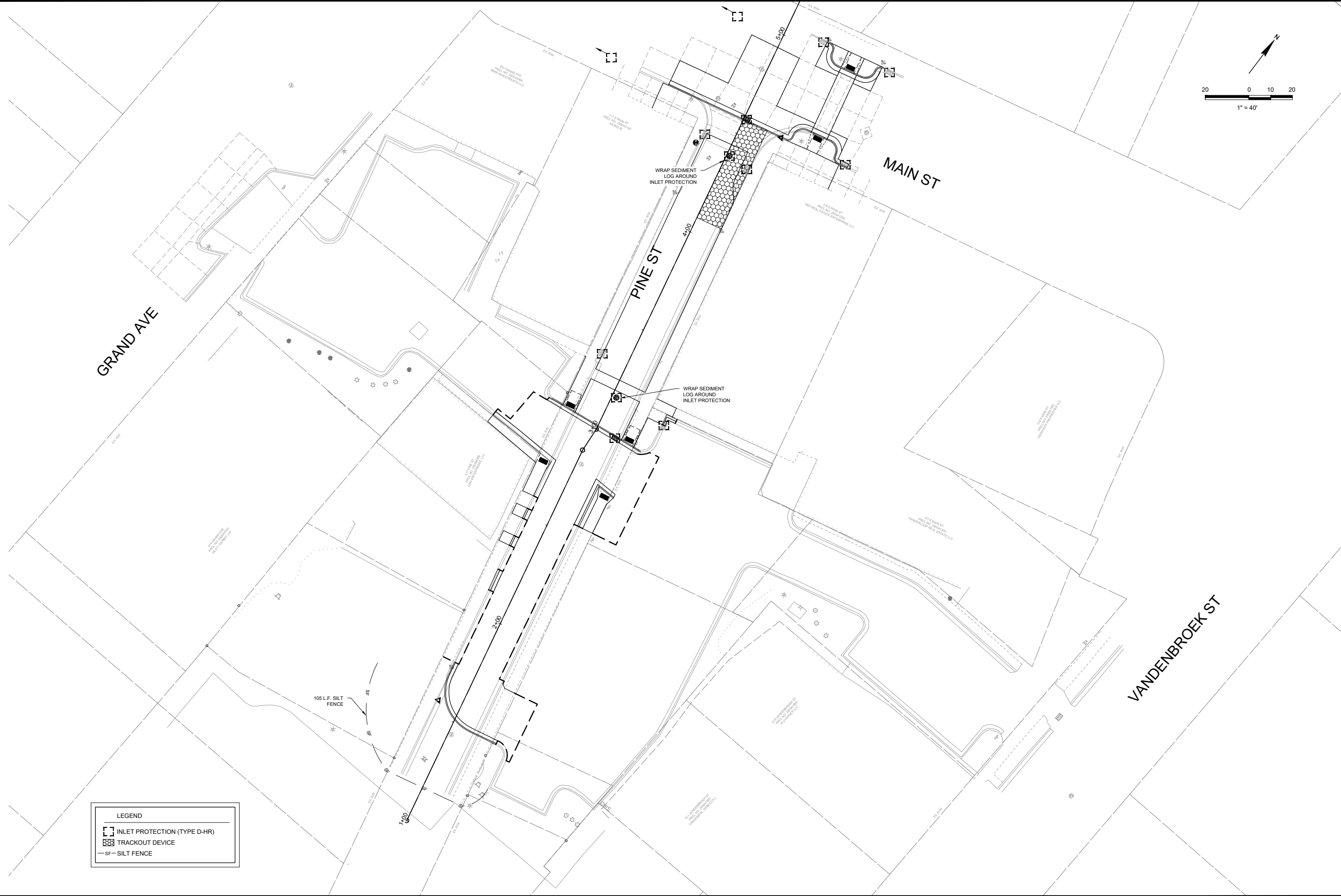
DESIGNED: KAL
DRAWN: KAL
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

4.0

2022 PINE STREET RECONSTRUCTION
PINE STREET DEMOLITION PLAN
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

Q:\Data of Public Works\Engineering Department\2000 - Municipal Projects\Pine St - Main - Vacation\10 - DWGS\Sheets\7_0_Pine St_Erosion Control_Grass.dwg, 5/22/2022 2:35:50 PM, KRIS LYONS, addit@chute.c3d-2018.dtb



LEGEND

INLET PROTECTION (TYPE D-HR)

TRACKOUT DEVICE

SF - SILT FENCE

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED: KAL

DRAWN: KAL

CHECKED: REO

APPROVED: CLM

PROJECT NUMBER

2022003

SHEET REFERENCE NO.

6.0

2022 PINE STREET RECONSTRUCTION

PINE STREET EROSION CONTROL

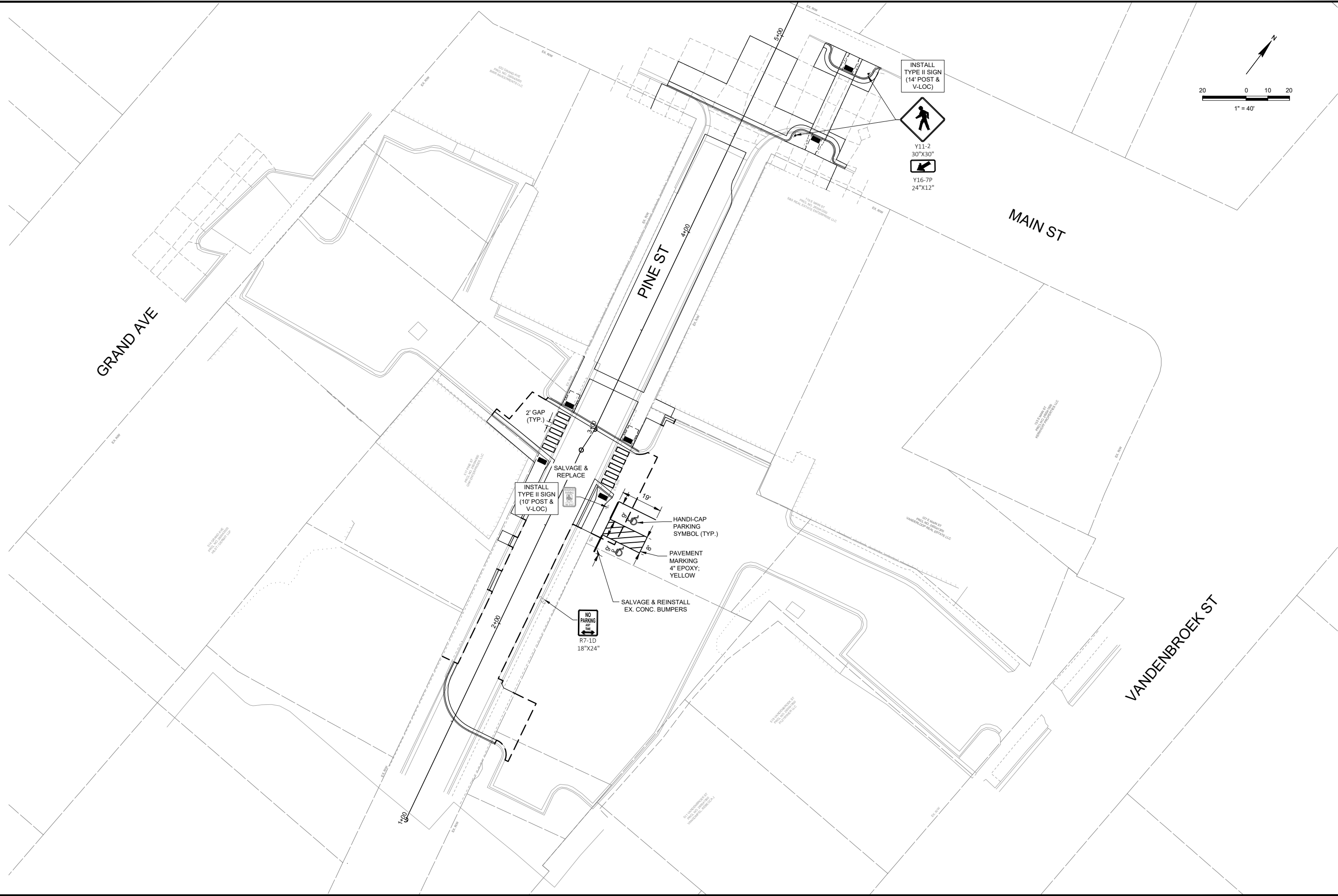
PINE STREET

VILLAGE OF LITTLE CHUTE, WISCONSIN

Little Chute
ESTABLISHED 1848

ENGINEERING DEPARTMENT
108 WEST MAIN STREET
LITTLE CHUTE, WISCONSIN 54140

Q:\Dept of Public Works\Engineering Department\2000 - Municipal Projects\Pine St - Main - Vacation\10 - DWGS\Sheets\8_0_Pine St Stripping Plan_Grass.dwg: 5/2/2022 2:36:09 PM: KRIS LYONS: gatliff@chute.c3d-2018.dtb



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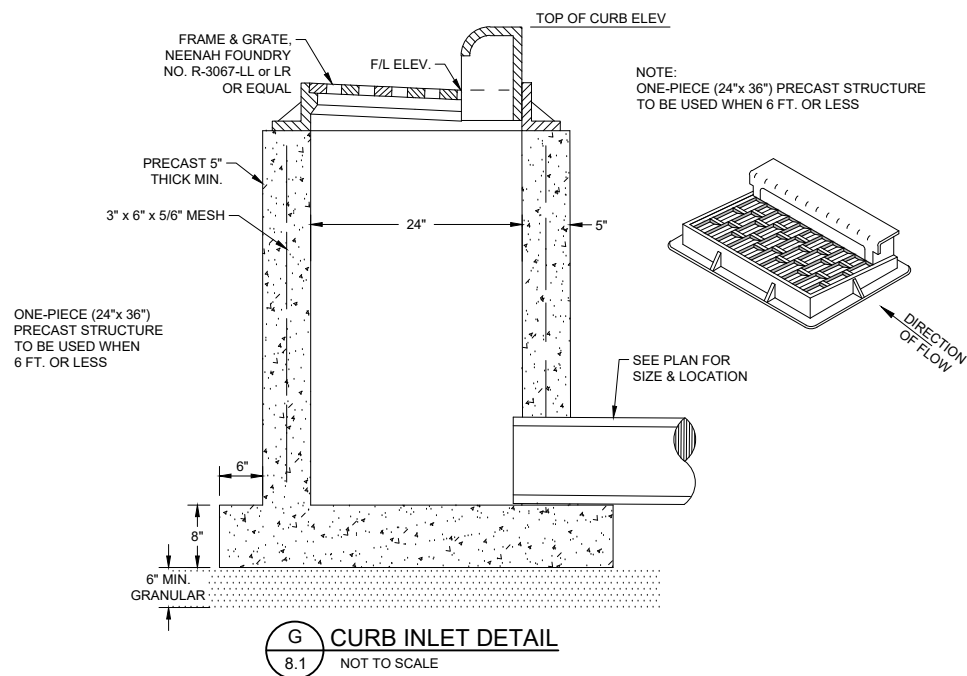
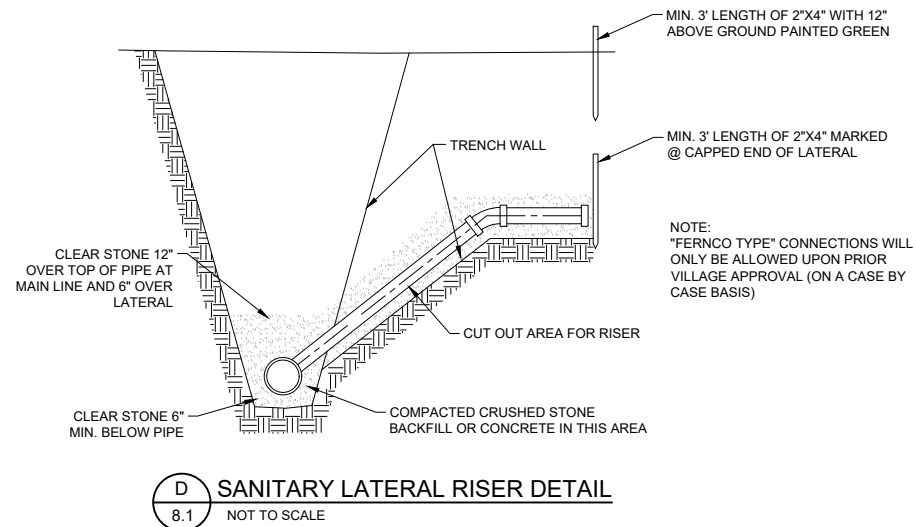
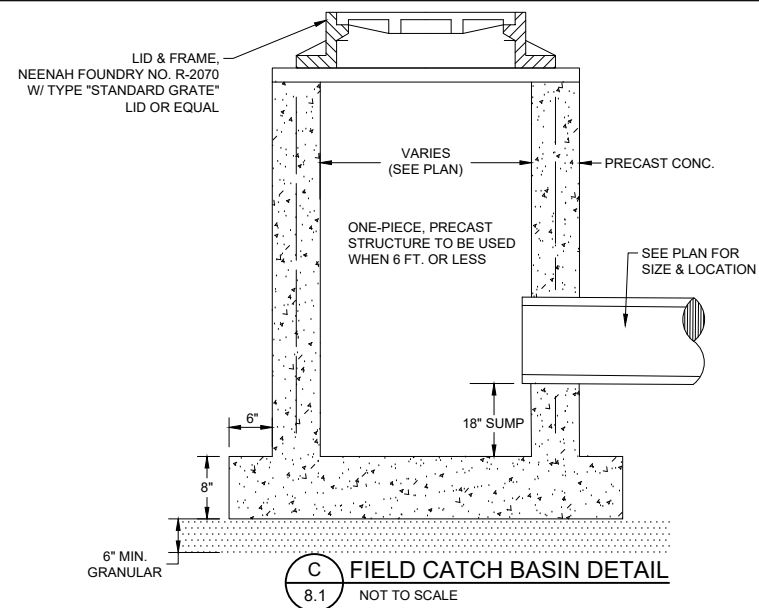
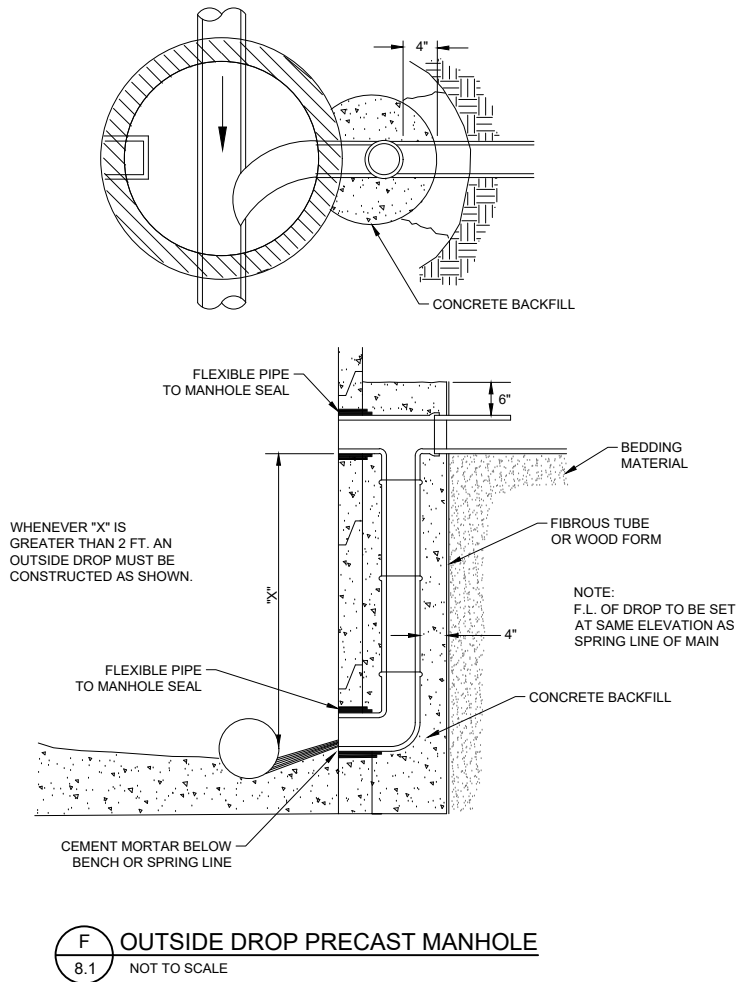
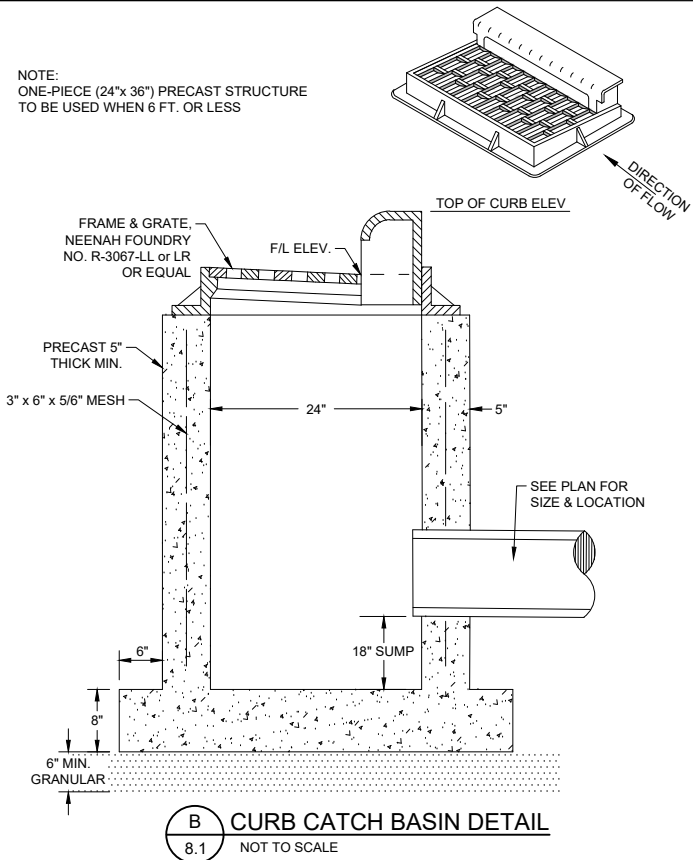
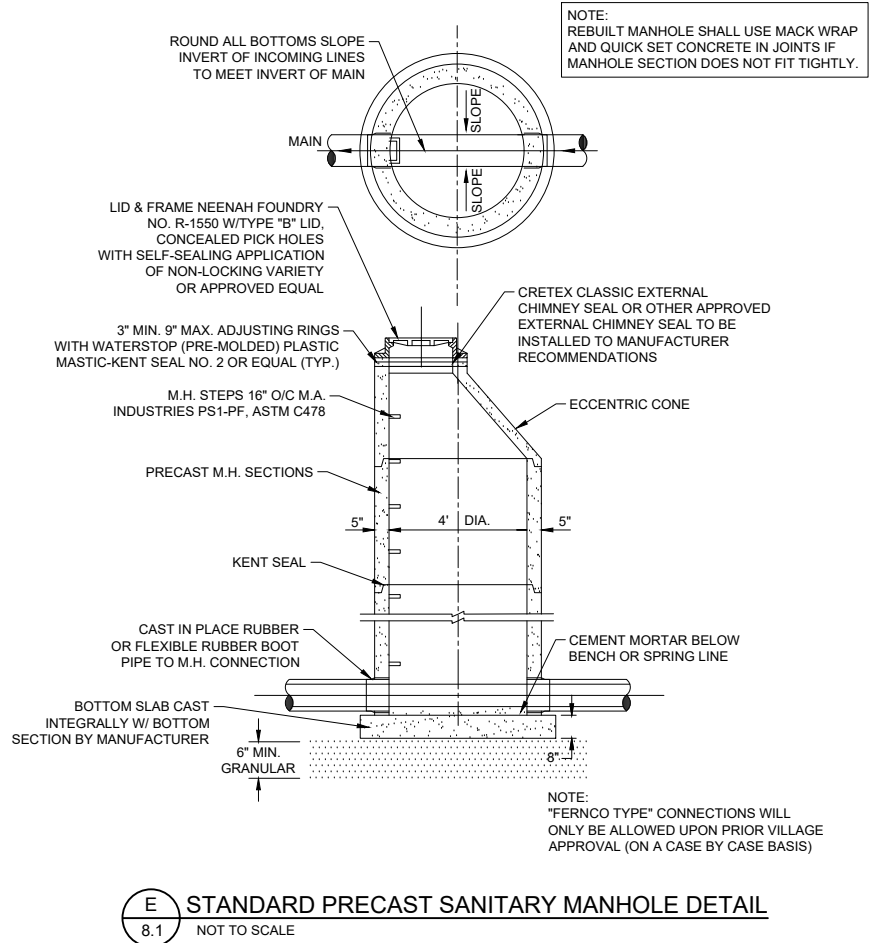
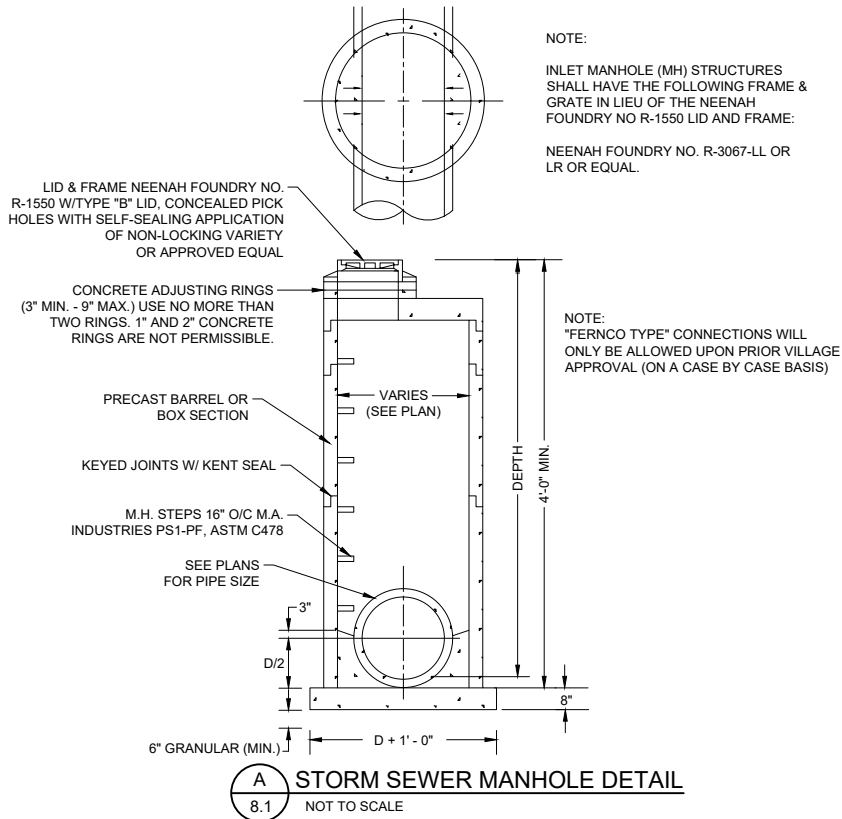
DESIGNED: KAL
DRAWN: KAL
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

7.0

2022 PINE STREET RECONSTRUCTION
PINE STREET STRIPING & SIGNAGE PLAN
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

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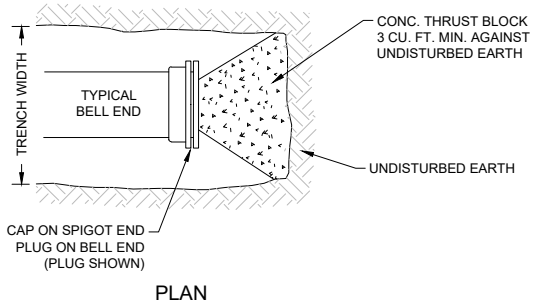
REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED:
DRAWN: DEM
CHECKED: REO
APPROVED: CLM

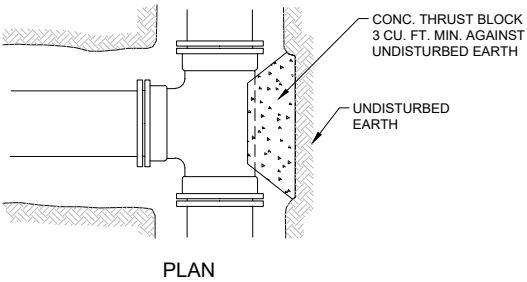
PROJECT NUMBER
2022003
SHEET REFERENCE NO.

MINIMUM RESTRAINT LENGTH (FT) ON BOTH SIDES OF THE FITTING				
FITTING TYPE/NOMINAL SIZE	6"	8"	12"	16"
11 1/4" BEND	2	2	3	3
22 1/2" BEND	3	3	5	6
45° BEND	5	6	9	11
90° BEND	11	15	21	27
DEAD END	30	40	56	73
TOP SIDE OF A VERTICAL OFFSET	13	17	24	31
TEE RUN X BRANCH 6"BY	14			
TEE RUN X BRANCH 8"BY	10	24		
TEE RUN X BRANCH 12"BY	1	15	40	
TEE RUN X BRANCH 16"BY	1	7	33	56

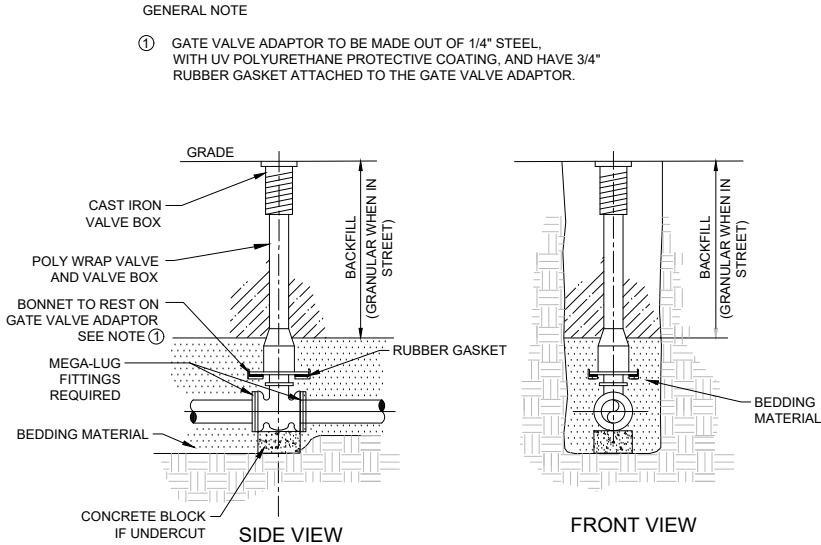
NOTES:
1. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 P.S.I AND ON AN EARTH RESISTANCE OF 2 TONS PER SQUARE FOOT.



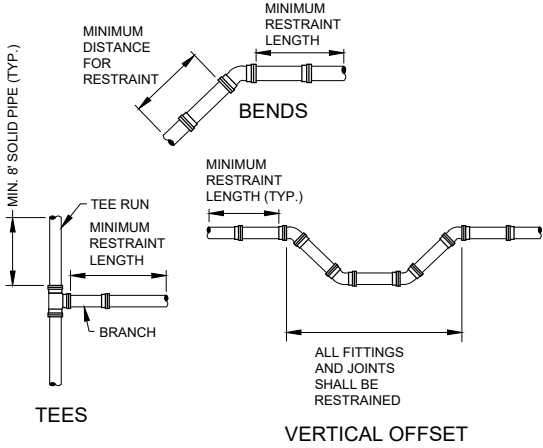
A BLOCKING FOR PLUGS
8.2 NOT TO SCALE



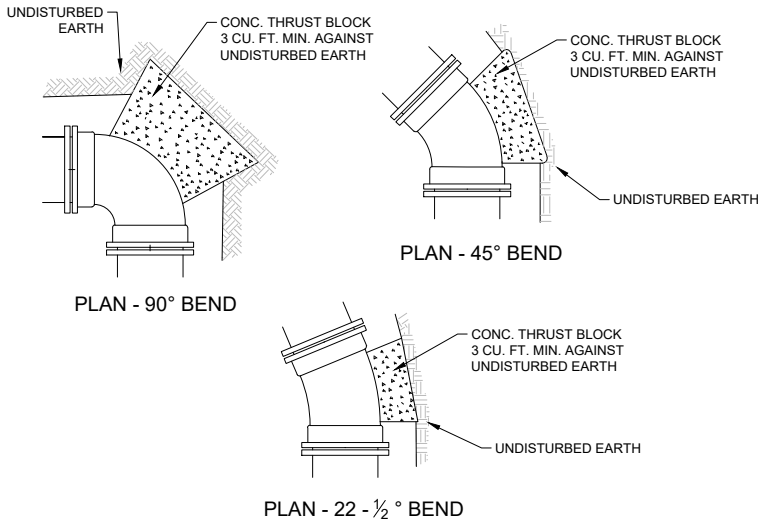
E BLOCKING FOR TEES
8.2 NOT TO SCALE



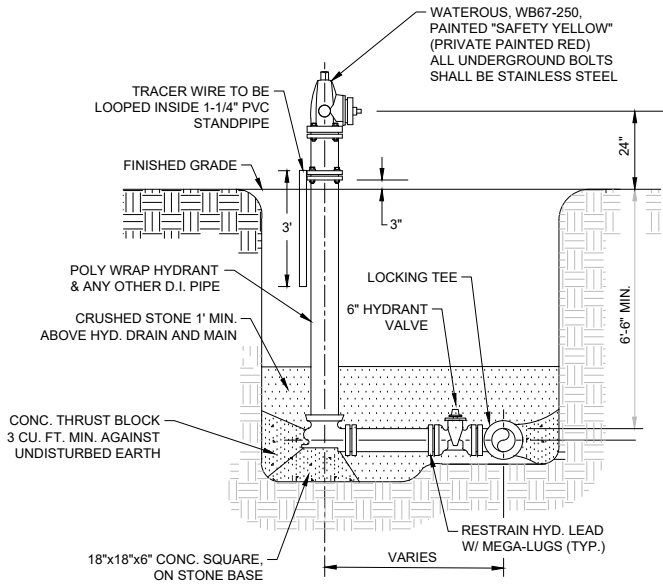
B STANDARD VALVE & VALVE BOX SETTING
8.2 NOT TO SCALE



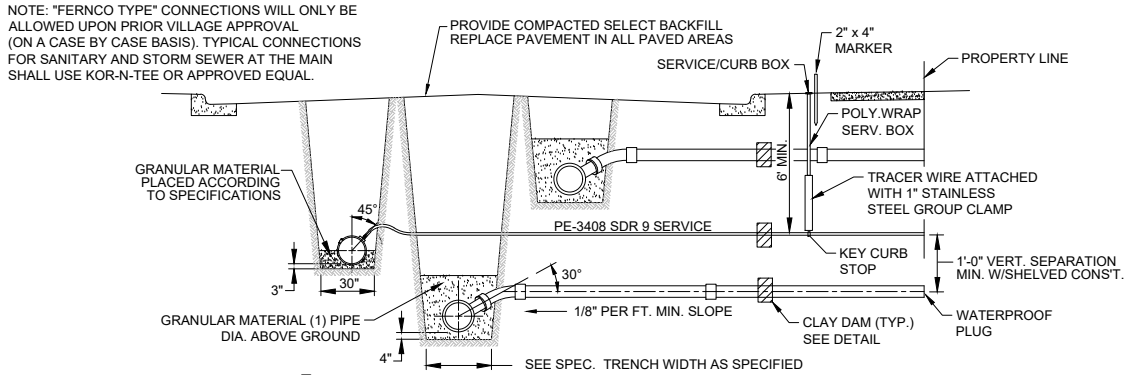
D WATER MAIN RESTRAINT DETAIL
8.2 NOT TO SCALE



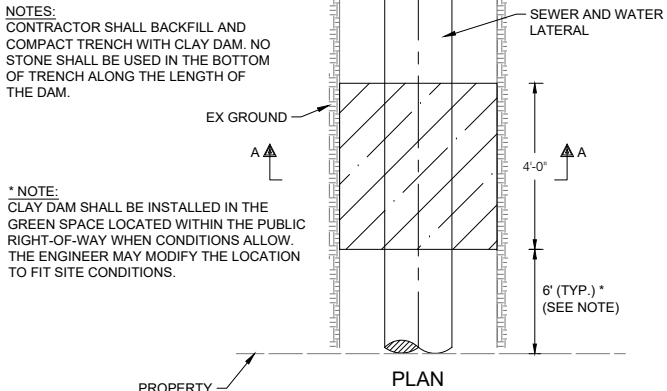
F BLOCKING FOR WATER MAIN BENDS
8.2 NOT TO SCALE



C HYDRANT DETAIL
8.2 NOT TO SCALE



G TYPICAL CONNECTION SEWER & WATER
10.2 NOT TO SCALE



H CLAY DAM
8.2 NOT TO SCALE

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GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

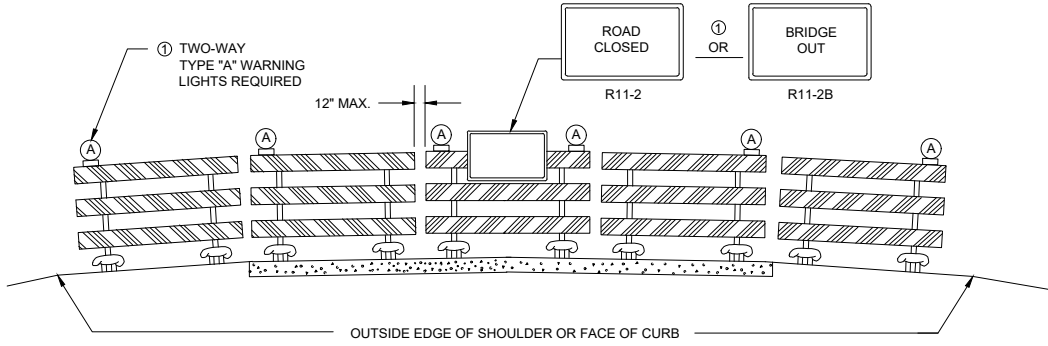
THE R11-2 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

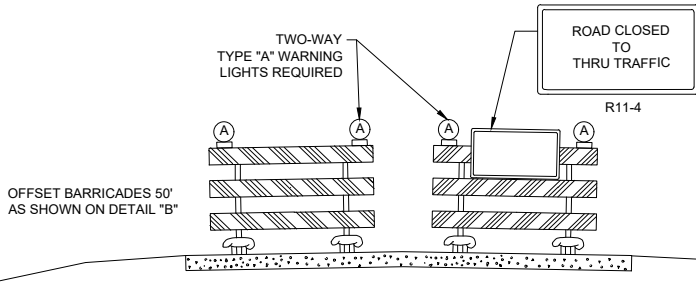
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30"
R11-4 SHALL BE 60" X 30"

1. TWO WARNING LIGHTS SHALL BE PROVIDE ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARING LIGHTS SHALL BE UNIFORM TO THE EDGE OF THE ROADWAY AS SHOWN (APPOX. 8-FOOT LIGHT SPACING)
2. THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURES BEGIN AT INTERSECTIONS.
3. FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE "DETAIL D".
4. FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT. SEE LANE CLOSURE BARRICADE "DETAIL E".
5. FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
6. "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



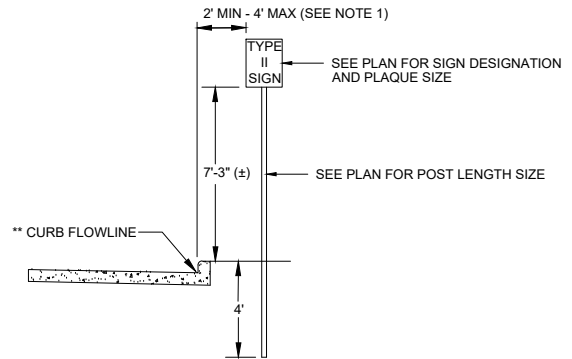
DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

A
8.3
TRAFFIC CONTROL, BARRICADE CLOSURES FOR LANES AND ROADS
NOT TO SCALE

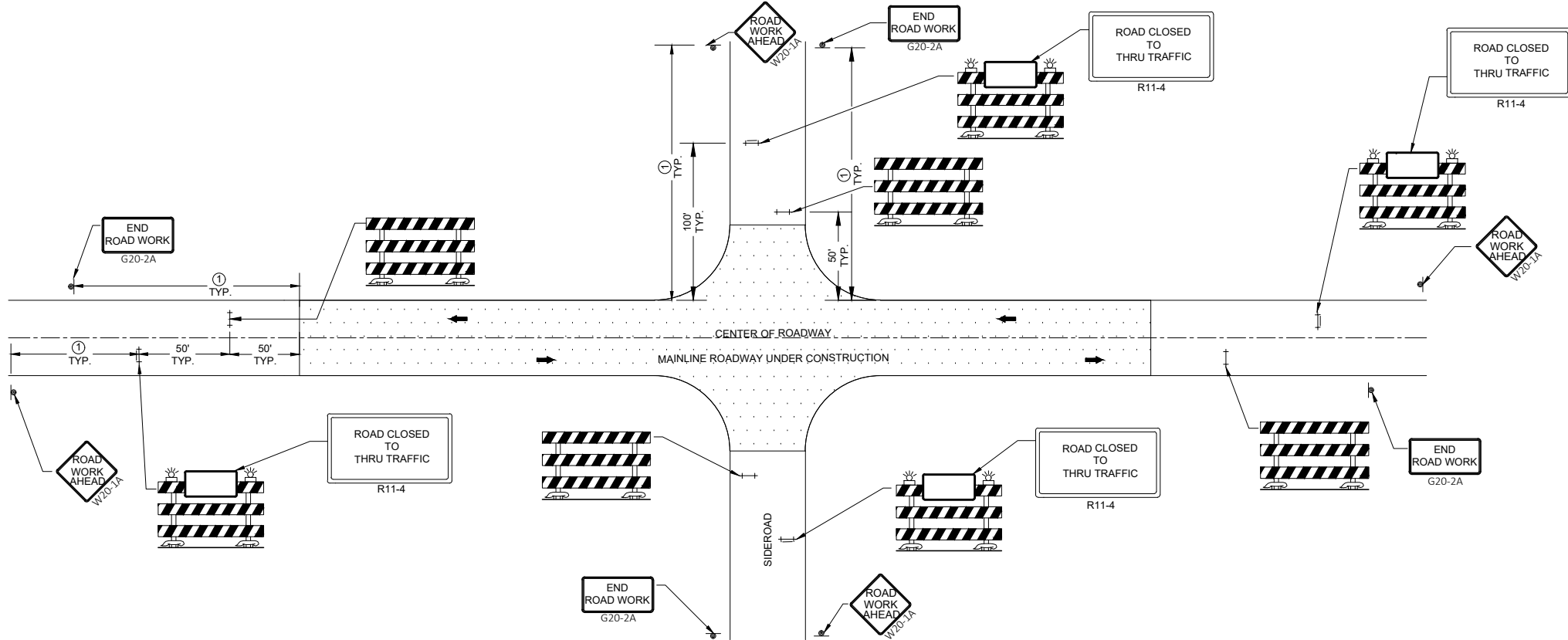
GENERAL NOTES

** IF SIDEWALK ADJACENT TO THE ROADWAY IS PRESENT VERTICAL CLEARANCE SHALL BE MEASURED TO THE SIDEWALK, NOT THE TOP OF CURB AS ILLUSTRATED.

1. OFFSET DISTANCE SHALL BE CONSISTENT WITH EXISTING SIGNS OR CONSISTENT THROUGHOUT LENGTH OF PROJECT
2. THE (±) TOLERANCE FOR MOUNTING HEIGHT IS 3 INCHES
3. SIGN POSTS SHALL BE GREEN NUCOR STEEL MARION RIB-BAK U-CHANNEL SIGN POSTS, 3.0LBS./FT.
4. TYPE II SIGNS SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE WIS DOT STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITIONS.



B
8.3
TYPE II SIGN INSTALLATION
NOT TO SCALE



C
8.3
TRAFFIC CONTROL, MAINLINE AND SIDE ROAD CLOSURE
NOT TO SCALE

GENERAL NOTES

ALL SIGNS SHALL BE AS NOTED BELOW:

G20-2A SHALL BE 36" X 18"
R11-4 SHALL BE 60" X 30"
W20-1A SHALL BE 36" X 36"

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED SIGNS"

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DETERMIND BY THE ENGINEER.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS. UNLESS APPROVED BY THE ENGINEER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

- ① 350' IF 35 - 40 MPH. OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
200' IF 25 - 30 MPH.

LEGEND

- ⬆ SIGN ON TEMPORARY OR PERMANENT SUPPORT
- ⬆ TYPE III BARRICADE
- ⬆ TYPE III BARRICADE WITH ATTACHED SIGN
- ➡ DIRECTION OF TRAFFIC
- ▨ WORKING AREA

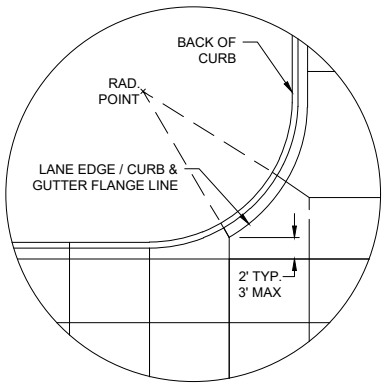
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	03/03/2022		ISSUED FOR BID

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APPROVED: CLM

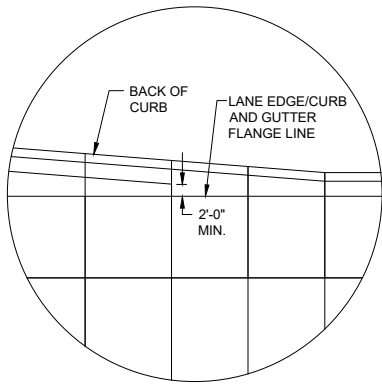
PROJECT NUMBER
2022003
SHEET REFERENCE NO.

8.3

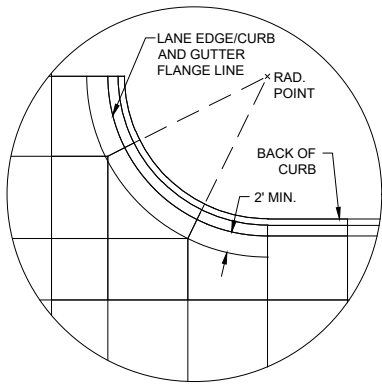
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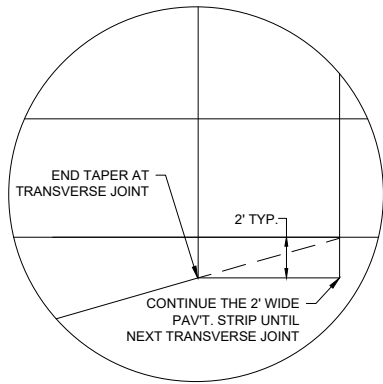
DETAIL "A"



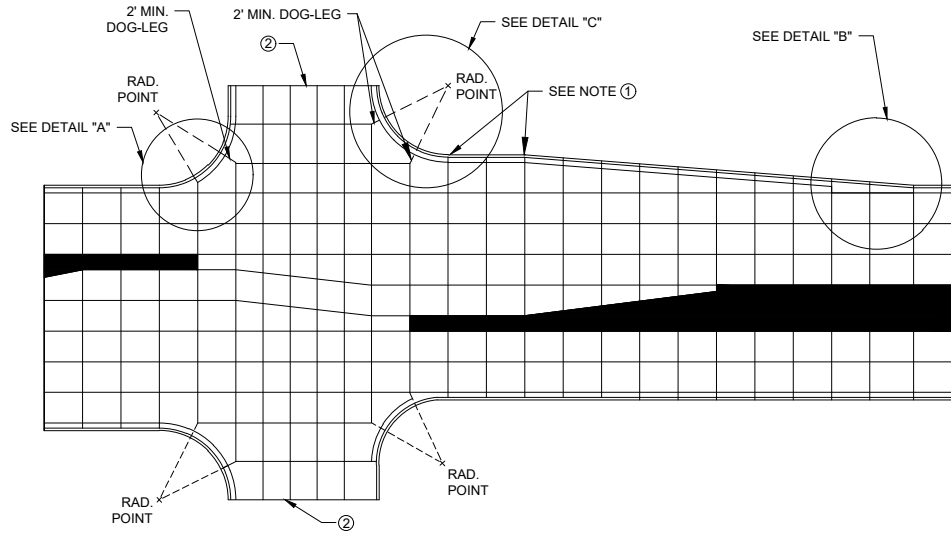
DETAIL "B"



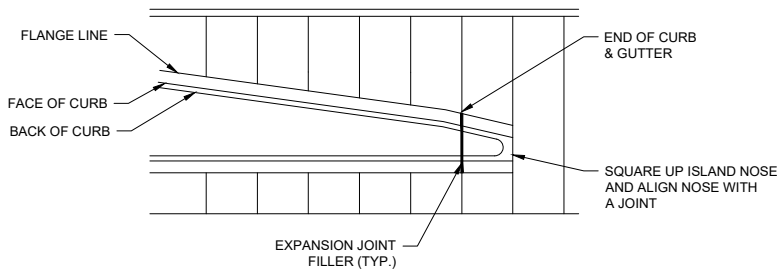
DETAIL "C"



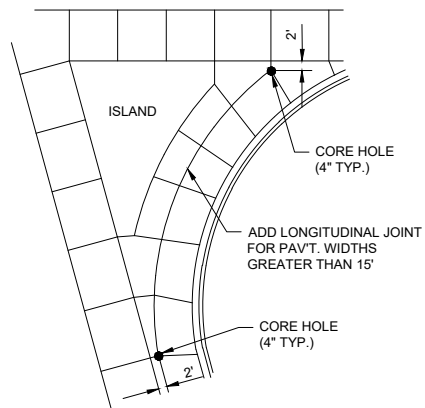
DETAIL "D"



STANDARD INTERSECTION

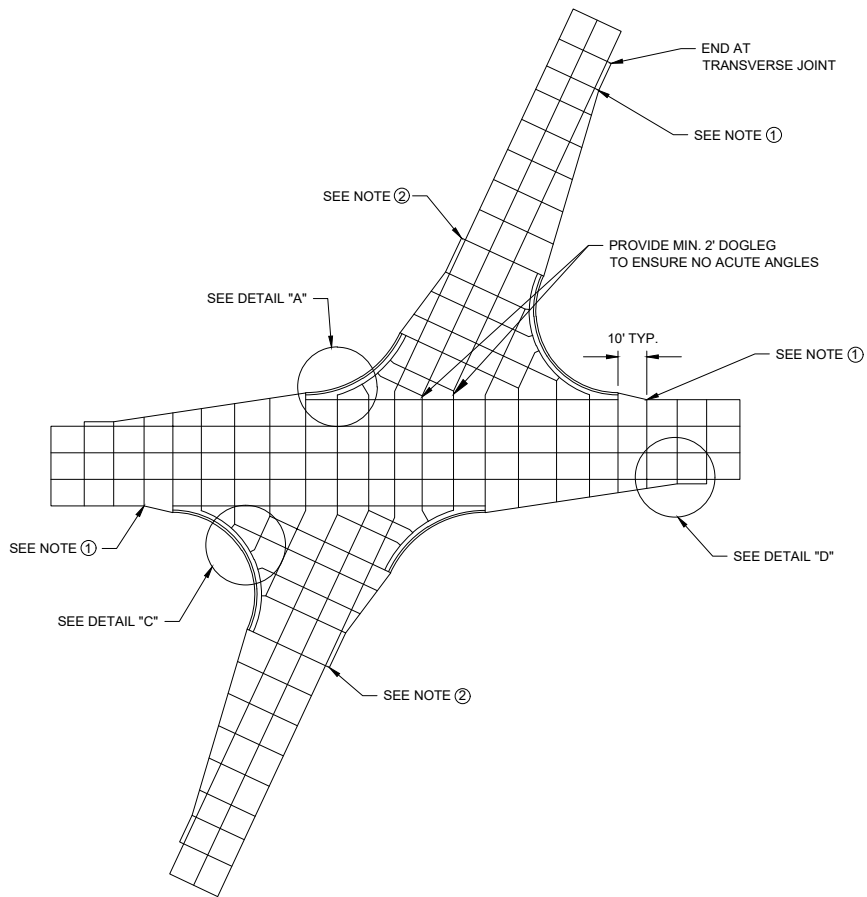


APPROACH TO MEDIAN



LARGE RIGHT TURN

PAVEMENT DEPTH AND JOINT SPACING TABLE	
PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6"-6 1/2"	12'
7"-7 1/2"	14'
8" & ABOVE	15'



SKEWED INTERSECTION

GENERAL NOTES

THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN.

ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.

CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.

ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G. MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.

AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.

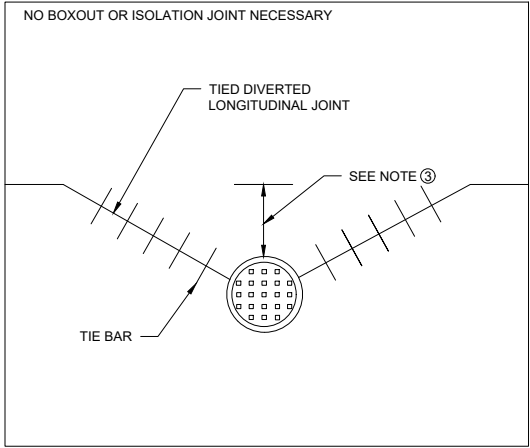
SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.

AVOID ANGLES LESS THAN 60° BY DOG-LEGGING JOINTS THROUGH CURVE RADIUS POINTS, USE 90° ANGLES WHEN POSSIBLE.

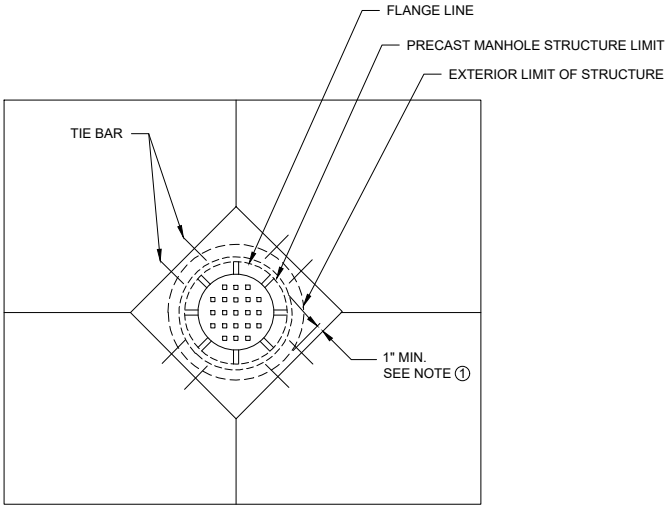
CORRELATE LONGITUDINAL JOINTS WITH LANES WHEN POSSIBLE.

1. PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.
2. CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300' IN LENGTH, ALIGN EXPANSION JOINT WITH EDGE OF RADIUS.
3. THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.

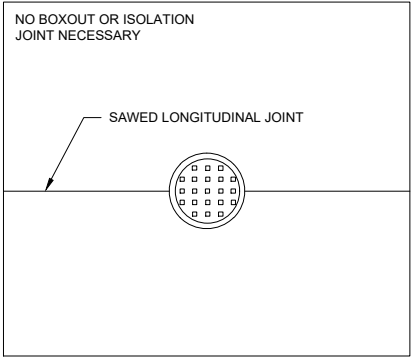
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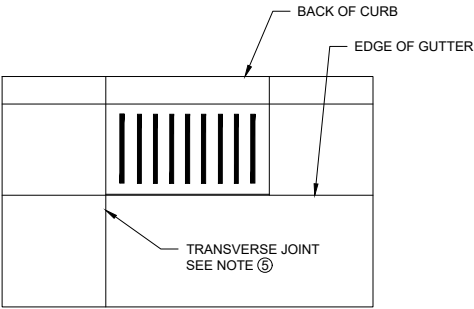
MANHOLE WITH
DIVERTED
LONGITUDINAL
CONTRACTION JOINT



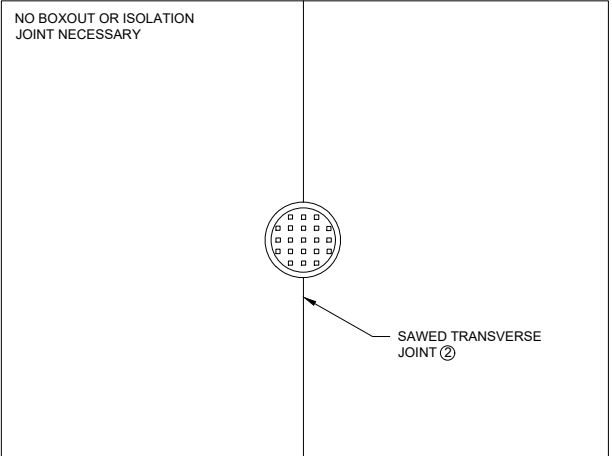
DIAGONAL MANHOLE
BOXOUT
FOR CONSTRUCTION
JOINTS



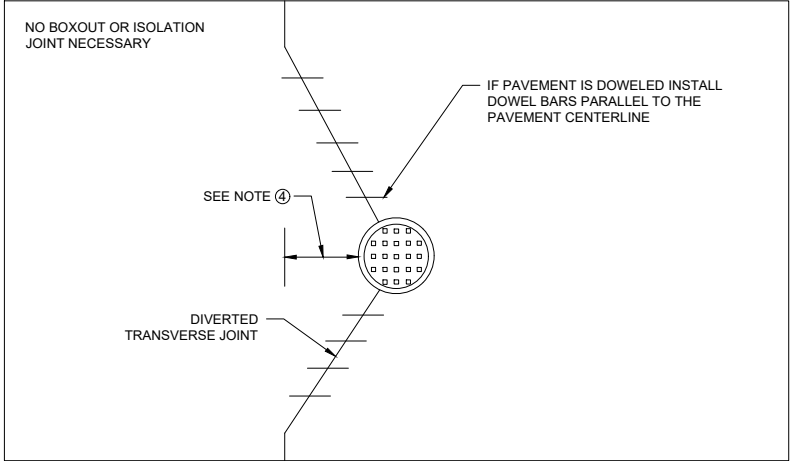
MANHOLE WITH
LONGITUDINAL JOINT



INLET WITH
TRANSVERSE JOINT



MANHOLE WITH
TRANSVERSE JOINT



MANHOLE WITH DIVERTED
TRANSVERSE CONTRACTION JOINT

GENERAL NOTES

1. USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1-FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
2. ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
3. IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
4. IF DISTANCE FROM EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
5. ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

A
8.5

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

NOT TO SCALE

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

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DRAWN: DEM
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

8.5

PLAN VIEW

SIDE VIEW
CONTRACTION JOINT DOWEL ASSEMBLY ①

DOWELED CONTRACTION JOINT

TRANSVERSE CONTRACTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE, AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5-1/2", 6", 6-1/2"	NONE	12'
7", 7-1/2"	1"	14'
8", 8-1/2"	1-1/4"	15'
9", 9-1/2"	1-1/4"	15'
10" & ABOVE	1-1/2"	15'

GENERAL NOTES

CONTRACTION JOINTS:

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE OF PAVEMENT.

CONSTRUCTION NOTES:

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

- ① OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER, AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- ③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4" RADIUS AT FORMED JOINTS.
- ④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- ⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO *DRILLED DOWEL BAR CONSTRUCTION JOINT* DETAIL.
- ⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ⑦ ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY, MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN THE DOWEL DIAMETER, 9 INCHES IN LENGTH.

DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦

CONTRACTION JOINT LOCATIONS

JOINT DETAIL

A URBAN DOWELED CONCRETE PAVEMENT

8.6 NOT TO SCALE

B **DETECTABLE WARNING FIELD**
8.6 NOT TO SCALE

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

PLAN VIEW

ELEVATION VIEW

C TRUNCATED DOMES - DETECTABLE WARNING PATTERN

8.6 NOT TO SCALE

PLAN VIEW

D CURB RAMP - TYPE 2

8.6 NOT TO SCALE

SECTION A-A

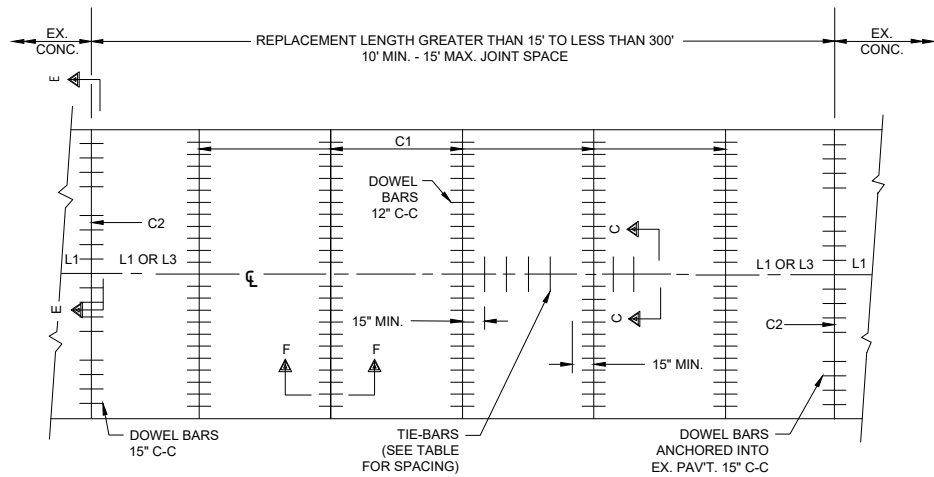
GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

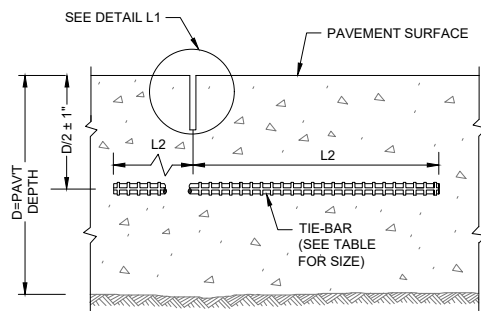
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ $\pm 0.5\%$ CONSTRUCTION SLOPE IN SIDEWALK CROSS SLOPE, THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN THIS DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

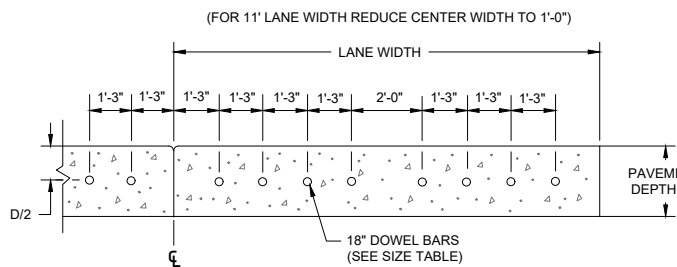
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PLAN VIEW
MULTI-LANE CONCRETE PAVEMENT REPLACEMENT

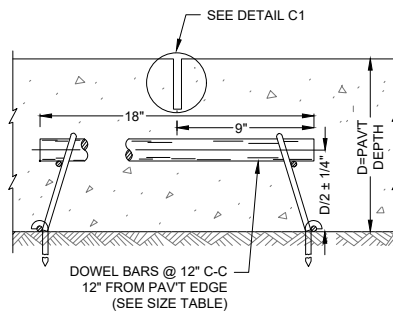


SECTION C-C
SAWED LONGITUDINAL JOINT

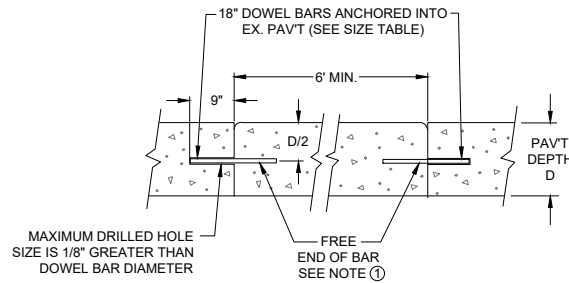


SECTION E-E
DRILLED DOWEL BAR CONSTRUCTION JOINT

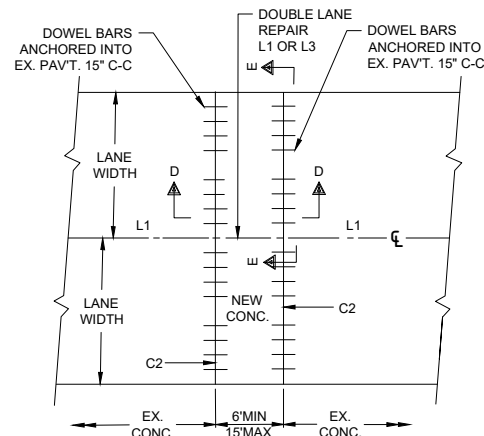
A MULTI-LANE CONCRETE REPLACEMENT
8.7 NOT TO SCALE



SECTION F-F
CONTRACTION JOINT



SECTION D - D



PLAN VIEW
MULTI - LANE CONCRETE PAVEMENT REPAIR

GENERAL NOTES:

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

CONCRETE PAVEMENT REPAIRS OF EXISTING NON-DOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

FOR MULTI - LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE-BAR NEAREST THAT JOINT OR EDGE.

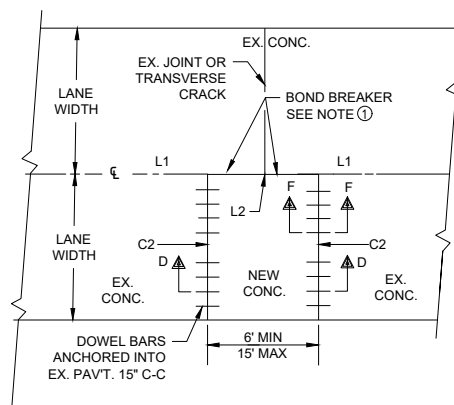
1. APPLY A THIN UNIFORM COATING SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.

PAVEMENT DEPTH, DOWEL BAR SIZE
AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONSTRUCTION JOINT SPACING
5 1/2", 6", 6 1/2"	NONE	12'
7", 7 1/2"	1"	14'
8", 8 1/2"	1 1/4"	15'
9", 9 1/2"	1 1/2"	15'
10" & ABOVE	1 3/4"	15'

TIE-BAR TABLE

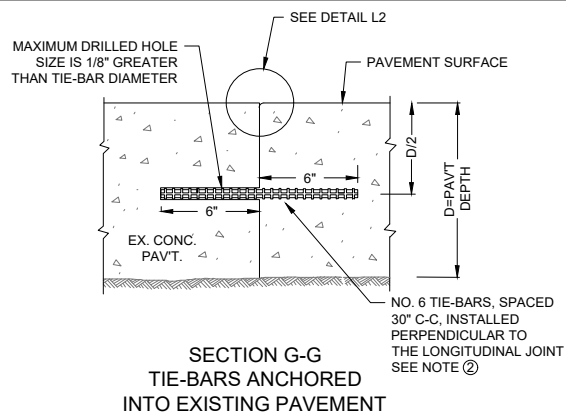
PAVEMENT DEPTH (D)	TIE-BAR SIZE	TIE-BAR LENGTH (L)	MAX. TIE-BAR SPACING
< 10 1/2"	NO. 4	30"	36"



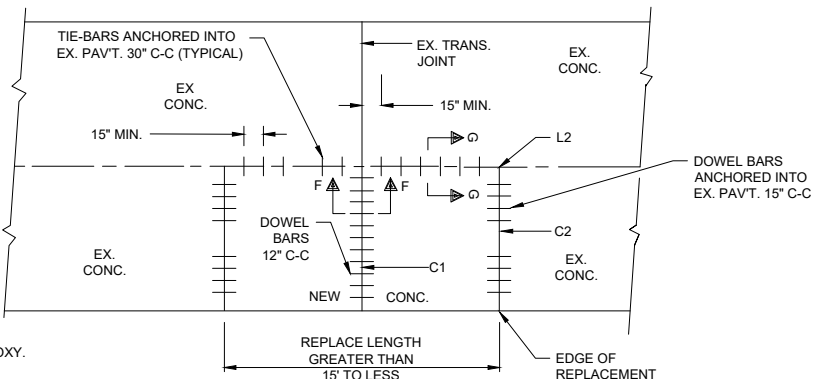
PLAN VIEW
SINGLE LANE
CONCRETE PAVEMENT REPAIR

GENERAL NOTES

1. USE AN ENGINEER - APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
2. ANCHOR TIE-BARS INTO DRILLED HOLES WITH AN EPOXY.

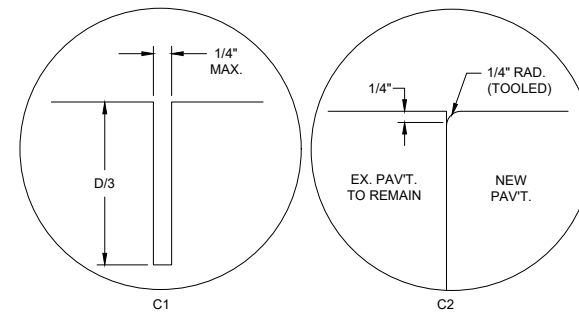


SECTION G-G
TIE-BARS ANCHORED
INTO EXISTING PAVEMENT

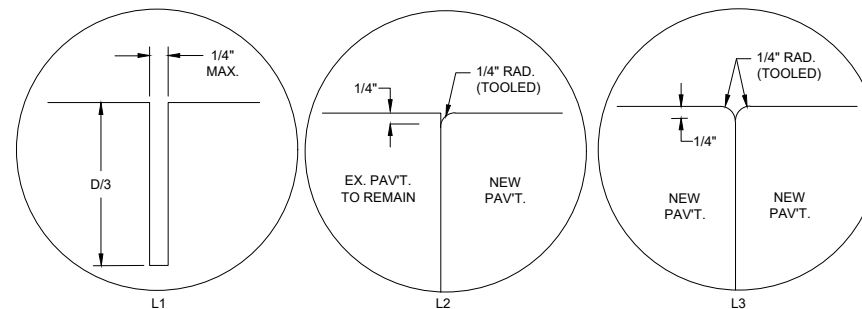


PLAN VIEW
SINGLE LANE CONCRETE REPLACEMENT

B SINGLE LANE CONCRETE REPLACEMENT
8.7 NOT TO SCALE



TRANSVERSE JOINTS



LONGITUDINAL JOINTS

C JOINTS DETAIL
8.7 NOT TO SCALE

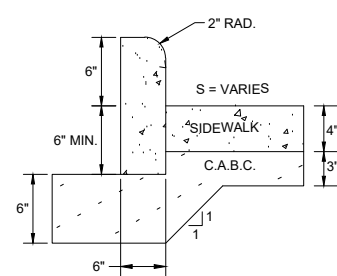
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-
- MAXIMUM GRADE CHANGE
BETWEEN APRON SLOPE AND
DRIVEWAY TRANSITION IS 15%
- TERRACE AREA
VARIES
- 5' SIDEWALK
- 0.5'
- R/W
- CURB & GUTTER
- DESIRABLE MIN.
SLOPE 1.5%
- SIDEWALK
SLOPE = 1.5% (TYP.)
- MAX GRADE 5%
(OR EX SLOPE IF GREATER THAN 5%)
DRIVEWAY TRANSITION
(WHERE REQ'D)
- IN CUT
- IN FILL
- PAVEMENT
- CONCRETE APRON
VARIES (SEE PLAN)
- 6" OR 7" CONC.
DRIVEWAY
- 1-1/4" BASE AGGREGATE
DENSE (3" THICK)
- SECTION A-A

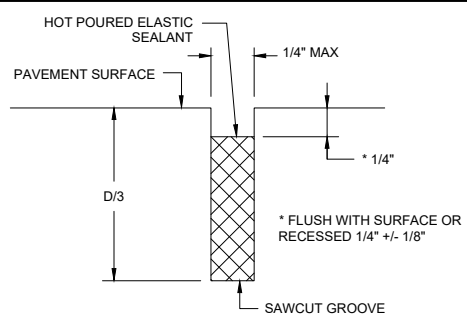
Diagram illustrating the cross-section of a sidewalk and its transition to a street. Key features and dimensions include:

- TOP OF CURB**: The upper edge of the sidewalk.
- PLANTING AREA**: The area between the curb and the street.
- 6"**: Dimension for the planting area width.
- 6" MIN.**: Minimum dimension for the planting area width.
- SIDEWALK TRANSITION AREA $S = 5.0\%$ (MAX.)**: The sloped area connecting the sidewalk to the street.
- ROUND OFF**: The top edge of the transition area.
- BACK EDGE OF SIDEWALK**: The edge of the sidewalk.
- SLOPE VARIES**: The sloped area connecting the sidewalk to the street.
- 1.5'**: Dimension for the transition area width.
- GUTTER FLOWLINE**: The line indicating the flow of water in the gutter.
- 1-1/2" MAX**: Maximum dimension for the gutter flowline.

CONCRETE DRIVEWAY



1 SIDEWALK CURB
8.8 NOT TO SCALE



JOINT SEALING DETAIL



HORIZONTAL REINFORCEMENT NO. 4 EPOXY COATED BARS. EACH BAR SHALL BE TIED TO EACH VERTICAL BAR WITH THE UPPER HORIZONTAL BAR TIED 3" FROM THE TOP OF THE VERTICAL BAR, AND THE LOWER HORIZONTAL BAR TIED 2" ABOVE THE TOP OF WALK. HORIZONTAL BARS SHALL BE LAPPED AND TIED. 12".

K CONCRETE RETAINING CURB DETAIL



-
- The diagram is an elevation view of a manhole. It shows a central rectangular structure labeled 'INLET COVER' with a dashed line labeled 'FLOW LINE' passing through its center. Above the cover, there are two 'A' markers with arrows pointing to the top of the cover structure. The top of the cover is labeled 'TOP OF CURB'. The edges of the pavement on either side are labeled 'EDGE OF PAVEMENT'. The joints of the pavement are labeled 'JOINT'. The horizontal distance from the centerline of the inlet cover to the edge of the pavement on both sides is labeled 'VAR. 3' NOR.'. The entire diagram is labeled 'ELEVATION' at the bottom.

Diagram illustrating the 1" DEPRESSION BELOW NORMAL FLOW LINE. The diagram shows a cross-section of a grate installed in a sewer. The grate is labeled "GRATE SHOWN IN SEWER". The flow line is indicated by an arrow labeled "FLOW LINE". The depression below the normal flow line is labeled "1" DEPRESSION BELOW NORMAL FLOW LINE". The distance from the flow line to the grate is labeled "VAR. 3" NOR". The distance from the grate to the sewer wall is labeled "VAR. 3" NOR". The label "(PAVEMENT)" is also present.

A cross-sectional diagram of a curb and gutter. The diagram shows a sloped pavement surface on the left, a vertical curb in the middle, and a gutter on the right. Labels with arrows point to the 'PAVEMENT SURFACE', 'FLAG LINE' (the top edge of the curb), and 'INLET COVER' (the top of the gutter). A double-headed arrow labeled 'VARIES' indicates the width of the gutter, which can be adjusted by changing the curb height.

L CURB AND GUTTER AT INLETS
8.8 NOT TO SCALE



MIN. THICKNESS

ASPHALTIC SURFACE

50' ±

SAW & REMOVE AT TERMINI

EX. ASPH. SURFACE

EX. PAV'T. OR STRUCT.

ASPHALTIC SURFACE REMOVAL

ASPHALTIC SURFACE WEDGING

BUTTED JOINT AT EXISTING JOINT

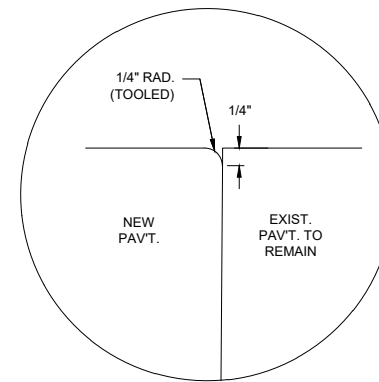
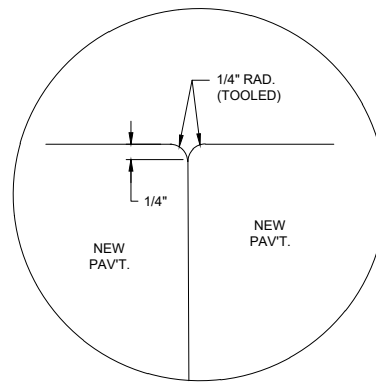
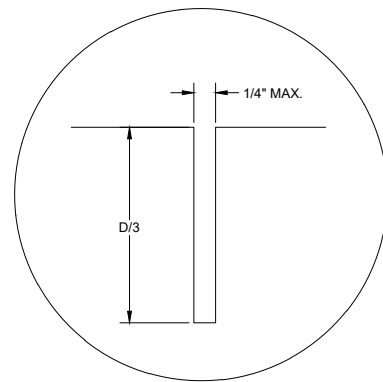
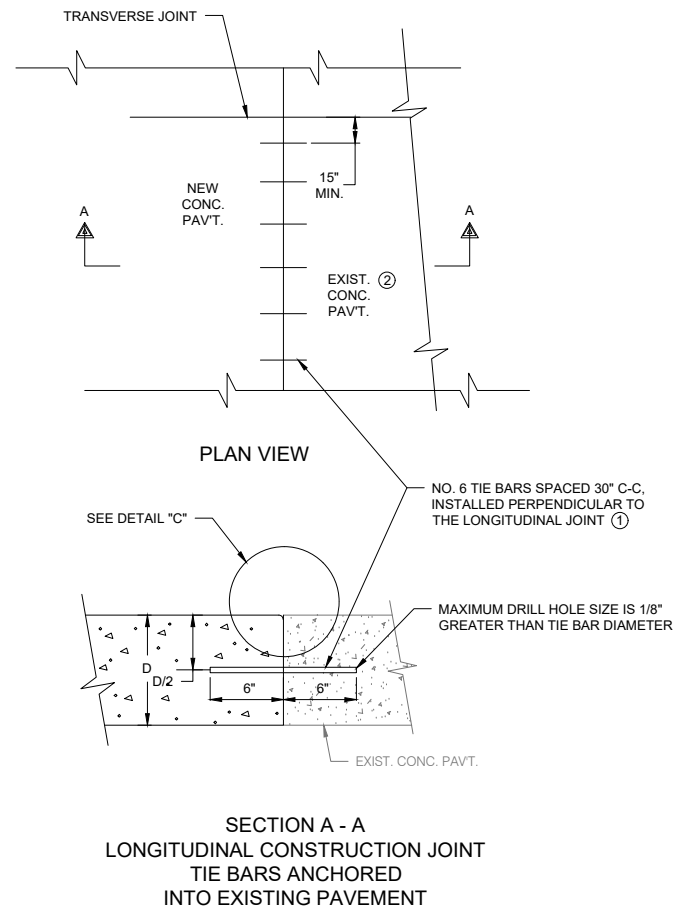
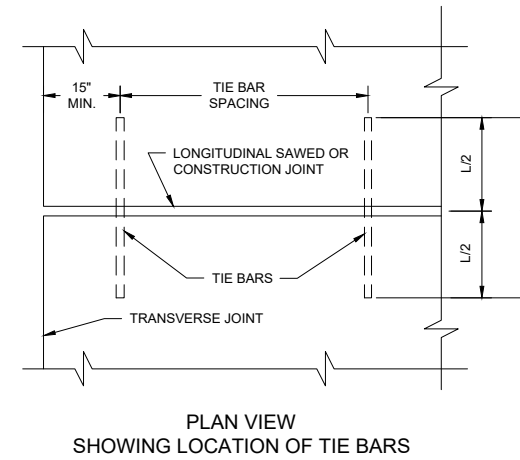
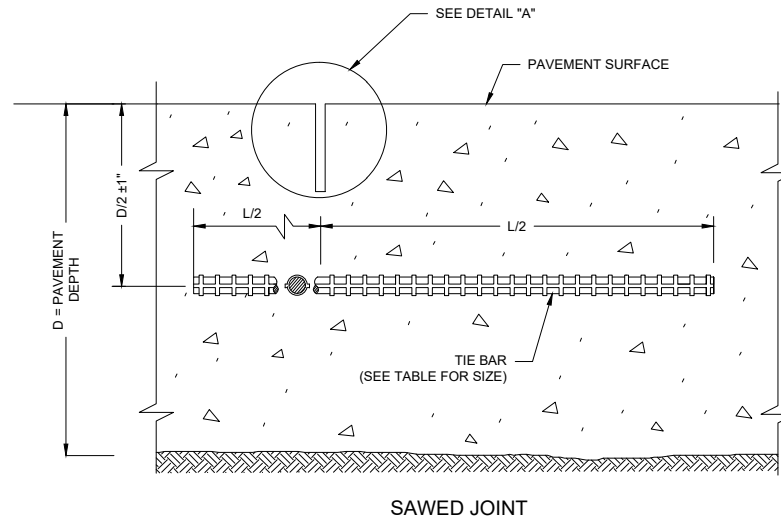
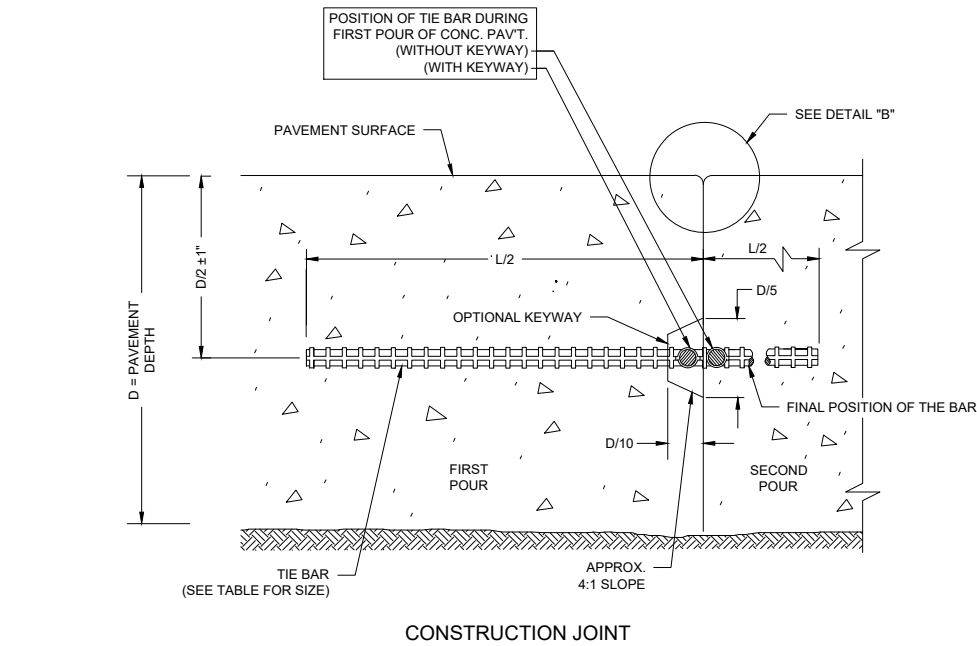
M OVERLAP JOINT, BUTTED DETAIL
8.8 NOT TO SCALE

-

TYPICAL ASPHALT PAVEMENT SECTION

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

Q:\Dept of Public Works\Engineering Department\2000 - Municipal Projects\Pine St. Main - Vacation\10 - DWGSHEETS\10.0 Pine St. Details - Grass.dwg, 5/2/2022 2:38:02 PM, KRIS LYONS, Little Chute-CSD-2018.dwg



TIE BAR TABLE			
PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
<10-1/2"	NO. 4	30"	36"
≥10-1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

GENERAL NOTES

CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

A LONGITUDINAL JOINTS AND TIES
8.9 NOT TO SCALE

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED:
DRAWN: DEM
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

8.9

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		BID # 1	BID # 2	BID # 3
		SANITARY MH	EXTERNAL	EXTERNAL CHIMNEY
		4' DIA V.F.	CHIMNEY SEAL EACH	SEAL EXTENSION EACH
PINE STREET	M-1	11.56	1	
PINE STREET	M-2	10.73	1	
PINE STREET	M-3	12.21	1	
PINE STREET		34.5	3	0
UNDISTRIBUTED		0	0	1
CONTRACT TOTALS		34.5	3	1

8" PVC Sanitary Sewer Main			
STRUCTURES			
	START	END	LENGTH
PINE ST	M-1	M-2	29.8
PINE ST	M-2	M-3	217.0
UNDISTRIBUTED			3.2
BID # 5 TOTALS			250.0

12" PVC Sanitary Sewer Main			
STRUCTURES			
	START	END	LENGTH
PINE ST	M-1	CO-1	5.0
PINE ST	M-1	CO-1	5.0
UNDISTRIBUTED			0.0
BID # 6 TOTALS			10.0

6" PVC Sanitary Sewer Lateral		
LATERAL		
		LENGTH
PINE ST	111 E. MAIN ST	24.0
PINE ST	119 E. MAIN ST	14.0
PINE ST	517 PINE ST	24.0
UNDISTRIBUTED		3.0
BID # 4 TOTALS		65.0

	BID # 7
	SANITARY
	ULO
	EACH
UNDISTRIBUTED	1.0
CONTRACT TOTAL	1.0

			BID # 8	BID # 9	BID # 10
			CONNECT	SANITARY	SANITARY
			TO EX.	CLAY	8"x6"
		#	SANITARY	DAM	WYE
PINE ST	CO-1	4+81 LT	1		
PINE ST	CO-2	4+81 RT	1		
111 E. MAIN ST		4+08 RT		1	1
119 E. MAIN ST		4+43 RT		1	1
517 PINE ST		2+37 RT		1	1
EASEMENT			2	3	3
UNDISTRIBUTED					
CONTRACT TOTALS			2	3	3

		BID # 11
		REMOVE
		EXISTING SAN.
SHEET #		MH
PINE STREET	2.0	2
PINE STREET		2
UNDISTRIBUTED		
CONTRACT TOTALS		2

		BID # 12	BID # 13
		SAND FILL	ABAN
		10" SAN.	EX. 10"
	STA.	STA.	L.F.
PINE ST	2+80	4+55	175
PINE ST	1+45	2+80	
PINE ST	4+65	4+80	
EASEMENT		175	150
UNDISTRIBUTED			
CONTRACT TOTALS		175	150

SANITARY SEWER QTY'S

6" PVC Water Main			
FITTINGS			
	START	END	LENGTH
PINE ST	R-1	B-1	1.5
PINE ST	B-1	B-2	1.5
PINE ST	B-2	V-2	1.4
PINE ST	V-2	H-2	7.8
PINE ST	T-1	V-1	1.4
PINE ST	V-1	H-1	2.0
UNDISTRIBUTED			0.4
BID # 15			16.0

8" PVC Water Main			
FITTINGS			
	START	END	LENGTH
PINE ST	CO-5	T-1	1.4
PINE ST	T-1	CO-6	15.9
UNDISTRIBUTED			2.7
BID # 16			20.0

1" Water Service		
SERVICE		
	ADDRESS	LENGTH
PINE ST	517 PINE ST	12.0
PINE ST	111 E. MAIN ST	8.0
PINE ST	119 E. MAIN ST	20.0
UNDISTRIBUTED		
BID # 17		20.0

		BID # 18	BID # 19	BID # 20
		CONNECT TO	1" WATER	WATER SERVICE
		EX. 8" WM	SERVICE	CLAY DAM
		STA.	EACH	EACH
PINE ST	CO-5	4+28 LT	1	
PINE ST	CO-6	4+45 LT	1	
PINE ST	CO-8	1+74 LT	1	
PINE ST	517 PINE ST		1	1
PINE ST	111 E. MAIN ST		1	1
PINE ST	119 E. MAIN ST		1	1
PINE ST		3	3	3
UNDISTRIBUTED				
CONTRACT TOTALS		3	3	3

				BID # 21
				6"
				VALVE
		STA.	OFFSET	EACH
PINE STREET	V-1	4+29	8.6 LT	1
PINE STREET	V-2	1+72	7.2 LT	1
PINE STREET				2
UNDISTRIBUTED				
CONTRACT TOTALS				2

				BID # 22
				6"
				45° Bend
		STA.	OFFSET	EACH
PINE STREET	B-1	1+72	4.7 LT	1
PINE STREET	B-2	1+71	5.9 LT	1
PINE STREET				2
UNDISTRIBUTED				2
CONTRACT TOTALS				4

				BID # 23
				8" X 6"
				TEE
		STA.	OFFSET	EACH
PINE STREET	T-1	4+29	6.8 LT	1
PINE STREET				1
UNDISTRIBUTED				
CONTRACT TOTALS				1

				BID # 24
				8" X 6"
				REDUCER
		STA.	OFFSET	EACH
PINE STREET	R-1	1+74	4.7 LT	1
PINE STREET				1
UNDISTRIBUTED				
CONTRACT TOTALS				1

				BID # 25
				FIRE
				HYDRANT
		STA.	OFFSET	EACH
PINE STREET	H-1	4+29	10.2 LT	1
PINE STREET	H-2	1+71	15.0 LT	1
PINE STREET				2
UNDISTRIBUTED				
CONTRACT TOTALS				2

		BID # 26	BID # 27
		ABANDON	REMOVE EX
		EXISTING 8" WM	HYDRANT
	STA.	STA.	L.F.
PINE STREET	1+29	1+70	41
PINE STREET	1+28		1
PINE STREET	4+38		1
PINE STREET		41	2
UNDISTRIBUTED			0
CONTRACT TOTALS		41	2

WATER MAIN QTY'S

2022 PINE STREET RECONSTRUCTION
PINE STREET QUANTITIES
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED: KAL
DRAWN: KAL
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

9.0



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4" PVC LATERAL			
LATERAL			
	ADDRESS	STATION	LENGTH
PINE STREET	119 E. MAIN ST	4+33	21.0
UNDISTRIBUTED			4.0
BID # 30 TOTALS			25.0

8" PVC LATERAL			
LATERAL			
	ADDRESS	STATION	LENGTH
PINE STREET	111 E. MAIN ST	3+32	23.0
UNDISTRIBUTED			2.0
BID # 31 TOTALS			25.0

12" PVC Storm Sewer Main			
STRUCTURES			
	START	END	LENGTH
PINE STREET	M-14	M-13	18.3
PINE STREET	CO-7	M-13	25.0
UNDISTRIBUTED			1.7
BID # 32 TOTALS			45.0

21" RCP Storm Sewer Main			
STRUCTURES			
	START	END	LENGTH
PINE STREET	M-10	M-11	7.1
PINE STREET	M-11	M-12	18
PINE STREET	M-12	M-13	122.6
UNDISTRIBUTED			2.3
BID # 33 TOTALS			150.0

36" RCP Storm Sewer Main			
STRUCTURES			
	START	END	LENGTH
PINE STREET	M-10	CO-3	5
PINE STREET	M-10	CO-4	5
UNDISTRIBUTED			
BID # 34 TOTALS			10.0

		BID # 35	BID # 36	BID # 37	BID # 38	BID # 39	BID # 40	BID # 41
		STORM MH	STORM INLET	2'x3'	2'x3' CATCH BASIN	ADJUST EX.	REPLACE EX.	REPLACE EX.
		5' DIA	MH 4' DIA	STORM INLET	W/ SUMP	STORM STRUCTURE	MH CASTING	INLET CASTING
		V.F.	EACH	EACH	EACH	EACH	EACH	EACH
PINE STREET	M-10	9						
PINE STREET	M-11			1				
PINE STREET	M-12		7.98					
PINE STREET	M-13		8.32					
PINE STREET	M-14				1			
PINE STREET		9	16.3	1	1			
UNDISTRIBUTED					2	1	1	
CONTRACT TOTALS		9	16.3	1	1	2	1	1

			BID # 42	BID # 43	BID # 44	BID # 45
			CONNECT	CONNECT	CONNECT	STORM SEWER
			TO EX.	TO EX.	TO EX.	LATERAL
		STA.	STORM SEWER	STORM INLET	STORM LATERAL	CLAY DAM
PINE STREET	CO-3	4+63	1			
PINE STREET	CO-4	4+63	1			
PINE STREET	CO-7	2+98		1		
PINE STREET	111 E. MAIN ST				1	1
PINE STREET	119 E. MAIN ST				1	1
PINE STREET			2	1	2	2
UNDISTRIBUTED						
CONTRACT TOTALS			2	1	2	2

REMOVE EX. 30" OR SMALLER STORM			
STATION			
	START	END	LENGTH
PINE STREET	INLET	TIL0345	40
PINE STREET	TIL0345	TIL1660	108
PINE STREET	TIL1660	TMH0597	15
PINE STREET	TIL1661	TIL1660	24
UNDISTRIBUTED			3
BID # 46 TOTALS			190

REMOVE EX. INLET			
	STRUCTURE	STA.	EACH
PINE STREET	TIL0345	3+32 LT	1
PINE STREET	TIL1660	4+33 RT	1
PINE STREET	TIL1661	4+44 LT	1
UNDISTRIBUTED			
BID # 47 TOTALS			3

	BID # 48
	12"
	BULKHEAD
	EACH
UNDISTRIBUTED	0.0
CONTRACT TOTAL	1.0

	BID # 49
	STORM
	ULO
	EACH
UNDISTRIBUTED	1.0
CONTRACT TOTAL	1.0

		BID # 97	BID # 98	BID # 99	BID # 100
		SILT	12" SEDIMENT	INLET PROTECTION	TRACKOUT
		FENCE	LOG	TYPE D-HR	CONTROL
	SHEET	L.F.	L.F.	EACH	EACH
PINE STREET	6.0	105	15	13	1
PINE STREET		105	15	13	1
UNDISTRIBUTED		5	20	1	0
CONTRACT TOTALS		110	35	14	1

STORM SEWER QTY'S

PAVING QTY'S

	BID # 51	BID # 52	BID # 53	BID # 54	BID # 55	BID # 56	BID # 57	BID # 58	BID # 59	BID # 60	BID # 61	BID # 62	BID # 63	BID # 64	BID # 65	BID # 66	BID # 67	BID # 68	BID # 69	BID # 70	BID # 71	BID # 72	
	Construction Staking (Grading / Paving)	4" Concrete Sidewalk, Including 3" Aggregate Base; Complete as specified	6" Concrete Driveway / Sidewalk, Including 3" Aggregate Base; Complete as specified	Furnish and Install No. 4 Reinforcing Rods; Complete as specified	Furnish and Install 24"x48" Detectable Sidewalk / Driveway Bars (#4x12"); Complete as specified	Furnish and Install 24"x48" Warning Field; Complete as specified	Remove Existing Concrete Curb & Gutter; Complete as specified	Install 24" Concrete Curb & Gutter - Standard; Complete as specified	Install 24" Concrete Curb & Gutter - Mountable; Complete as specified	Install 30" Concrete Curb & Gutter - Mountable; Complete as specified	Sawcut Asphalt Pavement; Complete as specified	Sawcut Concrete Pavement; Complete as specified	Asphalt Pavement Removal; Complete as specified	Concrete Pavement Removal; Complete as specified	Furnish and Install Base Aggregate Dense 1-1/4"; Complete as specified	Furnish and Install HMA Pavement 4 LT 58-28 S (2-1/4" Asphalt Base Course); Complete as specified	Furnish and Install HMA Pavement 4 LT 58-28 S (1-3/4" Asphalt Surface Course); Complete as specified	Furnish & Install Tack Coat; Complete as specified	Remove & Replace 9" Integral Concrete Pavement, Including 8" Aggregate Base; Complete as specified	Remove & Replace 9" (DOT Red) Integral Concrete Pavement, Including 8" Aggregate Base; Complete as specified	Furnish and Install Tie Bars (#6) ; Complete as specified	Furnish and Install Drilled Dowel Bars (1½"x 18") ; Complete as specified	
	L.S.	S.F.	S.F.	L.F.	EACH	EACH	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	S.Y.	S.Y.	C.Y.	S.Y.	S.Y.	GALLONS	S.Y.	S.Y.	EACH	EACH	
PINE STREET	1.0	3672.0	304.0	180.0	26.0	6.0	444.0	131.0	7.0	69.0	158.0	366.0	884.7	703.6	155.0	514.4	514.4	15.4	180.1	29.4	40.0	136.0	PNE STREET
UNDISTRIBUTED	-	53.0	26.0	20.0	4.0	0.0	11.0	5.0	3.0	5.0	7.0	9.0	0.3	0.4	2.0	0.6	0.6	0.1	0.9	0.1	5.0	4.0	UNDISTRIBUTED
CONTRACT TOTAL	1.0	3725.0	330.0	200.0	30.0	6.0	455.0	136.0	10.0	74.0	165.0	375.0	885.0	704.0	157.0	515.0	515.0	15.5	181.0	29.5	45.0	140.0	CONTRACT TOTAL

	BID # 73	BID # 74	BID # 75	BID # 76	BID # 77	BID # 78	BID # 79	BID # 80	BID # 81	BID # 82	BID # 83	BID # 84	BID # 85	BID # 86	BID # 87	BID # 88	BID # 89	BID # 90	BID # 91	BID # 92	BID # 93	BID # 94	BID # 95	BID # 96	
	Common Excavation; Complete as specified	Furnish and Install 3" Break Run; Complete as needed	Furnish and Install Geotextile Fabric, Type HR Non-Woven; Complete as needed	6" Topsoil, 6" Clean Soil Below Topsoil, Seed, Fertilizer, & Urban Class 1 Type A Erosion Mat; Complete as specified	Landscape Fabric & 3" of Hardwood Mulch; Complete as specified	Salvage and Reinstall Ex. Landscape Stone (New Fabric to be included); Complete as specified	Furnish and Install Temporary Pavement (Main Street); Complete as specified	Watering (For Compaction and Dust Control)	Remove Existing Pavement Marking, 4"; Complete as specified	Pavement Marking, Epoxy, 24"; Complete as specified	Pavement Marking, Epoxy, 24"; Complete as specified	Remove Existing Pavement Marking, Diagonal, 4"; Complete as specified	Pavement Marking, Diagonal, Epoxy, 4"; Complete as specified	Remove Existing Pavement Marking Handicap Symbol; Complete as specified	Pavement Marking Handicap Symbol, Epoxy; Complete as specified	Remove Existing Street Signs; Complete as specified	Remove Existing Street Signs & Post; Complete as specified	Salvage Ex. Street Signs & Post; Complete as specified	Furnish and Install 10-foot Sign Post; complete as specified	Furnish and Install 14-foot Sign Post; complete as specified	Furnish and Install V-Loc Sign Post; Complete as specified	Furnish and Install Type II Sign; complete as specified	Install Salvaged Type II Sign; complete as specified	Salvage & Reinstall Concrete Bumper; complete as specified	
	C.Y.	C.Y.	S.Y.	S.Y.	S.Y.	S.Y.	S.Y.	L.S.	L.F.	L.F.	L.F.	L.F.	L.F.	E.A.	E.A.	EA	EA	EA	EACH	EACH	EACH	EACH	EACH	EACH	
PNE STREET	397.5	-	-	613.1	29.6	5.7	240.0	1.0	44.0	73.0	50.0	48.0	60.0	3.0	2.0	3.0	2.0	1.0	1.0	2.0	3.0	3.0	1.0	2.0	PNE STREET
UNDISTRIBUTED	2.5	50.0	150.0	0.9	0.4	0.3	-	-	1.0	-	-	2.0	-	-	-	-	-	-	-	-	-	-	-	-	UNDISTRIBUTED
CONTRACT TOTAL	400.0	50.0	150.0	614.0	30.0	6.0	240.0	1.0	45.0	73.0	50.0	50.0	60.0	3.0	2.0	3.0	2.0	1.0	1.0	2.0	3.0	3.0	1.0	2.0	CONTRACT TOTAL

2022 PINE STREET RECONSTRUCTION
PINE STREET QUANTITIES
PINE STREET
VILLAGE OF LITTLE CHUTE, WISCONSIN

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED: KAL
DRAWN: KAL
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO.

9.1



Dept. of Public Works
108 W. Main Street
Little Chute, WI 54140

STORM SEWER COMPUTATIONS FOR:

Project Number:	PINE STREET PARKING LOT
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Sheet No.	1	of	1
Computed By:	KAL		
Date:	4/27/2022		

[illegible]

DATE	BY	DES
03/03/2022		ISSUED FOR BID

DESIGNED: KAL
DRAWN: KAL
CHECKED: REO
APPROVED: CLM

PROJECT NUMBER
2022003
SHEET REFERENCE NO

10.0

