



# AGENDA

## LITTLE CHUTE VILLAGE BOARD COMMITTEE OF THE WHOLE MEETING

PLACE: Little Chute Village Hall  
DATE: Wednesday, May 11, 2022  
TIME: 6:00 p.m.

**Virtually attend the May 11, Committee of the Whole meeting at 6 PM by following the zoom link here:**

Join Zoom Meeting

<https://us06web.zoom.us/j/83651057664>

Meeting ID: 836 5105 7664

Dial by your location: +1 312 626 6799 US (Chicago)

### REGULAR ORDER OF BUSINESS

- A. Invocation
- B. Roll call of Trustees
- C. Roll call of Officers and Department Heads
- D. Public Appearance for Items Not on the Agenda
  
- E. Discussion—Pine Street Plan
  
- F. Call for Unfinished Business
  
- G. Items for Future Agenda
  
- H. Adjournment

Requests from persons with disabilities who need assistance to participate in this meeting or hearing should be made with as much advance notice as possible to the Clerk's Office at 108 West Main Street, (920) 423-3852, [email: Laurie@littlechutewi.org](mailto:Laurie@littlechutewi.org) Prepared: May 6, 2022



## Item For Consideration

**For Board Review On:** May 4 2022

**Agenda Item Topic:** Pine Street Plan Discussion

**Prepared On:** May 6 2022

**Prepared By:** Admin. Bernhoft

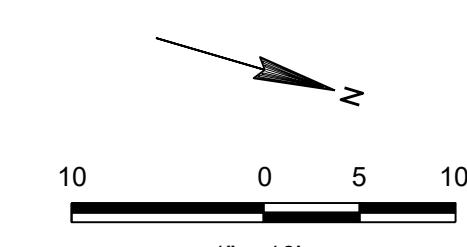
**Report:** The plans below are the updates requested by the Village Board for the Pine Street pedestrian mall project for your review. These changes check the primary boxes of the project needs such as utilities, closing of Pine Street for vehicle traffic and curb cleanup to Main Street and the south end of the property. Due to the changes depicted in the plans, it is important to give the public and local business owners an opportunity to review and provide feedback on the scope of work. With the work to be completed as presented, we will need to plan future renovations to the Village property such as pavement and access through the CIP process.

**Fiscal Impact:** Cost estimates for this project come in at \$321,743.88 (see below for breakdown). The current budget on Pine Street is \$1,190,072 (TID 8 Project), but those costs covered a full reconstruction of the existing parking lot with stormwater management.

**Recommendation/Board Action:** No action is needed at this meeting. This is an opportunity for the public stakeholders to review and provide comments on the project as presented. The Board can direct staff to make changes or continue as presented.

Respectfully Submitted,

Beau Bernhoft, Village Administrator





EXISTING SYMBOLS	EXISTING FEATURES LEGEND	EROSION CONTROL LEGEND	PROPOSED SYMBOLS	PROPOSED FEATURES LEGEND	STANDARD ABBREVIATIONS
BENCHMARK CONTROL POINT MAILBOX STORM MANHOLE SANITARY MANHOLE ELECTRIC MANHOLE WATER VALVE HYDRANT GAS VALVE ELECTRICAL BOX or PEDESTAL TELEPHONE BOX or PEDESTAL FIBER OPTIC PEDESTAL POWER POLE LIGHT POLE SIGN WATER MANHOLE TELEPHONE MANHOLE GUY WIRE CLEAN OUT CONIFEROUS TREE or BUSH DECIDUOUS TREE or BUSH STUMP CURB STOP STORM SEWER CATCH BASIN or INLET RIPRAP SECTION CORNER IRON PIPE PK NAIL CULVERT END SECTION LIGHT POLE W/ MAST ARM AIR CONDITIONER BOLLARD FLAG POLE MONITORING WELL SPRINKLER HEAD SIGNAL BOX RAILROAD SIGNAL FLASHER TRAFFIC SIGNAL COMMUNICATIONS VAULT ELECTRIC CABINET UTILITY METER FIBER OPTIC VAULT SOIL BORING DELINEATOR POST WATER SURFACE ELEVATION	CENTERLINE OR REFERENCE LINE SANITARY SEWER MAIN SANITARY SEWER LATERAL STORM SEWER MAIN STORM SEWER LATERAL STORM SEWER MINI SEWER WATER MAIN WATER SERVICE OVERHEAD UTILITY UNDERGROUND TELEPHONE UNDERGROUND ELECTRIC UNDERGROUND FIBER OPTIC UNDERGROUND GAS LINE UNDERGROUND COMMUNICATION UNDERGROUND TELEVISION PAVEMENT EDGE GRAVEL EDGE DITCH CENTERLINE BUILDING LINE CHAINLINK FENCE GENERIC FENCE WOOD FENCE BRUSHLINE UTILITY EASEMENT DELINATED WETLANDS CONCRETE CURB AND GUTTER CONCRETE JOINT GUARDRAIL WATER EDGE SIDEWALK MAJOR CONTOUR MINOR CONTOUR DRIVEWAY SLOPE TOP or BOTTOM TREELINE RIGHT-OF-WAY RAILROAD TRACKS	EROSION MAT CLASS I, TYPE A OR B EROSION MAT CLASS II OR III (EXCEPT II, TYPE A) RESTORATION W/ HYDROMULCH SILT FENCE TURBIDITY BARRIER TRACKOUT DEVICE SURFACE WATER FLOW EROSION BALE, BARRIER TEMPORARY DITCH CHECK RIPRAP EXISTING INLET INLET PROTECTION (TYPE D-HR) PROPOSED INLET INLET PROTECTION (TYPE D-HR) CULVERT PIPE CHECK	STORM MANHOLE OPEN GRATE INLET 2x3' INLET TYPE L GRATE APRON ENDWALL SANITARY MANHOLE CLEAN OUT WATER VALVE HYDRANT WATER MANHOLE CURB STOP CURB AND GUTTER INTEGRAL CURB AND GUTTER FENCE CURB AND GUTTER CATCH BASIN COMMERCIAL ENTRANCE CHORD CENTER LINE CLASS OF PIPE CORRUGATED METAL PIPE CONTROL POINT NAIL SIGN ASPHALT CONCRETE SIDEWALK CONCRETE DRIVEWAY / RAMP DETECTABLE WARNING PLATE REMOVE PIPE REMOVE TREE REMOVE UTILITY STRUCTURE ABANDON UTILITY STRUCTURE	CENTERLINE OR REFERENCE LINE SANITARY SEWER MAIN SANITARY SEWER LATERAL STORM SEWER MAIN STORM SEWER LATERAL WATER MAIN WATER SERVICE SLOPE INTERCEPT CONSTRUCTION LIMITS CURB AND GUTTER INTEGRAL CURB AND GUTTER FENCE MAJOR CONTOUR WATER MAIN 45° BEND WATER MAIN 22.5° BEND WATER MAIN 11.25° BEND WATER MAIN REDUCER CAP AND PLUG (UTILITY) SIGN ASPHALT CONCRETE SIDEWALK CONCRETE DRIVEWAY / RAMP DETECTABLE WARNING PLATE REMOVE PIPE REMOVE TREE REMOVE UTILITY STRUCTURE ABANDON UTILITY STRUCTURE	ABANDON ACRE APRON ENDWALL AGGREGATE ASPHALT PAVEMENT AVERAGE BURLY BOLT BACK CURB TO BACK CURB BEGIN BITUMINOUS BACK BASE LINE BUILDING BENCHMARK BACK OF CURB BEARING CRUSHED AGGREGATE BASE COURSE CENTER TO CENTER COUNTER CLOCKWISE CLOCKWISE CUBIC YARD CURB AND GUTTER CATCH BASIN COMMERCIAL ENTRANCE CHORD CENTER LINE CLASS OF PIPE CORRUGATED METAL PIPE CONTROL POINT NAIL CRUSHED CURB STOP CONCRETE SIDEWALK COUNTY TRUNK HIGHWAY CULVERT DEPTH OR DELTA DUCTILE IRON DIAMETER EACH EASTBOUND EXCAVATION BELOW SUBGRADE ECCENTRIC EDGE OF GRAVEL ELEVATION ELECTRIC EMBANKMENT EROSION MAT ENTRANCE END OF RADIUS EDGE OF PAVEMENT EXCAVATION EXISTING FACE CURB TO FACE CURB FOUNDATION FIELD ENTRANCE FERTILIZER FINISHED GRADE FLOW LINE FOOT FOOTING GRAVEL GRID NORTH GAS VALVE HIGH DENSITY POLYETHYLENE HOT MIX ASPHALT HIGH POINT HIGH PERFORMANCE POLYPROPYLENE HEIGHT HYDRANT INSIDE DIAMETER or INCH DIAMETER INCH INVERT IRON PIPE JUNCTION LATERAL POUND LINEAR FOOT LIGHT POLE LEFT LENGTH OF VERTICAL CURVE SHOULDER SURVEY LINE SQUARE STAINLESS STEEL STATION STANDARD STORM SIDEWALK SEVER TOP OF CURB TEMPORARY TEMPORARY LIMITED EASEMENT TELEVISION TYPICAL UNDERGROUND UTILITY LOCATE OPENING U.S. HIGHWAY VARIES VERTICAL CURVE PIPE VERTICAL WITH WESTBOUND WATER MAIN WATER VALVE

## GENERAL CONSTRUCTION NOTES

- THE UTILITIES SHOWN IN PLAN AND PROFILE ARE INDICATED IN ACCORDANCE WITH AVAILABLE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING EXACT LOCATIONS AND ELEVATIONS OF ALL UTILITIES, INCLUDING ANY PRIVATE UTILITIES, FROM THE OWNERS OF THE RESPECTIVE UTILITIES. ALL UTILITIES SHALL BE NOTIFIED 72 HOURS PRIOR TO EXCAVATION.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY PROPOSED SITE GRADES BY FIELD CHECKING TWO (2) BENCHMARKS AND A MINIMUM OF ONE (1) SITE FEATURE AS SHOWN ON THESE PLANS. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE VILLAGE OF ANY VERTICAL DISCREPANCY.
- ALL WORK PERFORMED IN THE RIGHT OF WAY, EASEMENTS, OR VACATED RIGHT OF WAY SHALL CONFORM TO THE VILLAGE OF LITTLE CHUTE PROJECT SPECIFICATIONS.
- THE PROPERTY LINES, RIGHT-OF-WAY LINES AND OTHER PROPERTY INFORMATION ON THIS DRAWING WERE DEVELOPED OR OBTAINED AS PART OF THE COUNTY GEOGRAPHIC INFORMATION SYSTEM OR THROUGH THE COUNTY PROPERTY TAX MAPPING FUNCTION THEREFORE, THIS INFORMATION DOES NOT GUARANTEE TO BE CORRECT, CURRENT OR COMPLETE. THE PROPERTY AND RIGHT-OF-WAY INFORMATION IS INTENDED FOR USE AS A GENERAL REFERENCE AND IS NOT INTENDED OR SUITABLE FOR SITE-SPECIFIC USES. ANY USE TO THE CONTRARY OF THE ABOVE STATED USES IS THE RESPONSIBILITY OF THE USER AND SUCH USE IS AT THE USER'S OWN RISK.
- NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT PRIOR APPROVAL FROM THE OWNER.
- A SAWED JOINT IS REQUIRED WHERE NEW ASPHALTIC PAVEMENT MATCHES EXISTING ASPHALTIC CONCRETE SURFACE.
- ALL CURB RADII SHOWN ON THE PLAN SHEETS ARE TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
- TRUCKS SHALL TAKE THE SHORTEST ROUTE TO THE NEAREST TRUCK ROUTE. ROUTES SHALL BE ON CONCRETE PAVEMENT ONLY, UNLESS OTHERWISE APPROVED BY THE VILLAGE ENGINEERING DEPARTMENT.

## EROSION AND SEDIMENT CONTROL PLAN

BEST MANAGEMENT PRACTICES:  
 THE CONTRACTOR IS RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING BEST MANAGEMENT PRACTICES IN ACCORDANCE WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES (DNR) TECHNICAL STANDARDS. THESE STANDARDS MAY BE FOUND ON THE DNR WEBSITE AT [https://www.dnr.wi.gov/topic/stormwater/standards/const\\_standards.html](https://www.dnr.wi.gov/topic/stormwater/standards/const_standards.html)  
 RIP-RAP SHALL BE IN ACCORDANCE WITH SECTION 608 OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, LATEST EDITION UNTIL TECHNICAL STANDARD 1065 IS COMPLETED BY THE DNR. THE MINIMUM BEST MANAGEMENT PRACTICES SPECIFIED FOR THIS PROJECT ARE AS FOLLOWS:

- LAND APPLICATION OF POLYACRYLAMIDE (1050)
- WATER APPLICATION OF ADDITIVES FOR SEDIMENT CONTROL (1051)
- NON-CHANNEL EROSION MAT (1052)
- CHANNEL EROSION MAT (1053)
- VEGETATIVE BUFFER (1054)
- SEDIMENT BAILE BARRIER (1055)
- SILT FENCE (1056)
- TRACKOUT CONTROL PRACTICES (1057)
- MULCHING (1058)
- SEEDING (1059)
- STORM DRAIN INLET PROTECTION (1060)
- DE-WATERING (1061)
- DITCH CHECK (1062)
- SEDIMENT TRAP (1063)
- CONSTRUCTION DIVERSION (1066)
- RIP-RAP (1065)
- GRADING PRACTICES (1067)
- DUST CONTROL (1068)
- TURBIDITY BARRIER (1069)
- SILT CURTAIN (1070)
- MANUFACTURED PERIMETER PRODUCTS (1071)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES AND IMPLEMENT BEST MANAGEMENT PRACTICES TO PREVENT OR REDUCE ALL OF THE FOLLOWING:

- DEPOSITION OF TRACTING SOIL ONTO STREETS BY VEHICLES.
- DISCHARGE OF SEDIMENT INTO STORM WATER INLETS.
- DISCHARGE OF SEDIMENT INTO ADJACENT STREAMS, RIVERS, LAKES AND WETLANDS.
- DISCHARGE OF SEDIMENT FROM DITCHES AND STORM SEWERS THAT FLOW OFFSITE.
- DISCHARGE OF SEDIMENT FROM DEWATERING ACTIVITIES.
- DISCHARGE OF SEDIMENT FROM SOIL STOCKPILES EXISTING FOR SEVEN (7) DAYS OR MORE.
- DISCHARGE OF SEDIMENT FROM EROSION OUTLET FLOWS.
- TRANSPORT OF CHEMICALS, CEMENT AND BUILDING MATERIALS BY RUNOFF.
- TRANSPORT OF UNTREATED VEHICLE AND WHEEL WASH WATER BY RUNOFF

THE CONTRACTOR SHALL IMPLEMENT THE FOLLOWING PREVENTATIVE MEASURES:

- PRESERVE EXISTING VEGETATION WHENEVER POSSIBLE.
- MINIMIZE SOIL COMPACTION AND PRESERVE TOPSOIL.
- MINIMIZE LAND DISTURBANCES ON SLOPES OF 20% OR MORE.
- MINIMIZE THE AMOUNT OF SOIL EXPOSED AT ANY ONE TIME.
- DIVERT CLEAR WATER AWAY FROM EXPOSED SOILS.
- TEMPORARILY STABILIZE EXPOSED SOILS THAT WILL NOT BE ACTIVE FOR 14 DAYS OR MORE. USE MULCHING, SEEDING, POLYACRYLAMIDE OR GRAVELING TO STABILIZE.
- PERMANENTLY STABILIZE EXPOSED SOILS AS SOON AS POSSIBLE.
- CONTRACTOR SHALL EDUCATE ITS EMPLOYEES AND SUBCONTRACTORS ABOUT PROPER SPILL PREVENTION AND RESPONSE PROCEDURES. IF A SPILL OCCURS, THE CONTRACTOR SHALL EVACUATE THE AREA AND IMMEDIATELY NOTIFY THE LOCAL MUNICIPALITY, FIRE DEPARTMENT OR 911 EMERGENCY SYSTEM. IF NO FIRE, EXPLOSION OR LIFE / HEALTH SAFETY HAZARD EXISTS, THE NEXT STEP IS TO CONTAIN THE SPILL AND PERFORM CLEANUP. USE DRY CLEANUP METHODS, NOT WET.

THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING OR REPLACING BEST MANAGEMENT PRACTICES DESTROYED AS A RESULT OF CONSTRUCTION ACTIVITIES BY THE END OF THE WORK DAY. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING BEST MANAGEMENT PRACTICES TEMPORARILY REMOVED FOR CONSTRUCTION ACTIVITY AS SOON AS THOSE ACTIVITIES ARE COMPLETED. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING AND DISPOSING OF TEMPORARY BEST MANAGEMENT PRACTICES AFTER CONSTRUCTION IS COMPLETE AND PERMANENT VEGETATION IS ESTABLISHED.

## INSPECTION &amp; MAINTENANCE:

THE CONTRACTOR IS RESPONSIBLE FOR INSPECTING BEST MANAGEMENT PRACTICES WEEKLY, AND WITHIN 24 HOURS FOLLOWING A RAINFALL OF 0.5 INCHES OR GREATER. WRITTEN DOCUMENTATION OF EACH INSPECTION SHALL BE KEPT AT THE CONSTRUCTION SITE AND SHALL INCLUDE THE FOLLOWING INFORMATION: DATE, TIME, AND LOCATION OF INSPECTION; NAME OF INDIVIDUAL WHO PERFORMED THE INSPECTION; AN ASSESSMENT OF THE CONDITION OF BEST MANAGEMENT PRACTICES; A DESCRIPTION OF THE ANY BEST MANAGEMENT PRACTICE IMPLEMENTATION AND MAINTENANCE PERFORMED; AND A DESCRIPTION OF THE PRESENT PHASE OF CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAIN, REPAIRING, OR REPLACING BEST MANAGEMENT PRACTICES AS NECESSARY WITHIN 24 HOUR OF AN INSPECTION OR NOTIFICATION. THE CONTRACTOR IS RESPONSIBLE FOR INSPECTION, MAINTAINING, REPAIRING, OR REPLACING BEST MANAGEMENT PRACTICES UNTIL ALL LAND DISTURBING ACTIVITY IS COMPLETED AND A UNIFORM PERENNIAL VEGETATIVE COVER IS ESTABLISHED WITH A DENSITY OF AT LEAST 70%.

THE CONTRACTOR IS RESPONSIBLE FOR POSTING THE PERMIT IN A CONSPICUOUS LOCATION ON THE CONSTRUCTION SITE. THE CONTRACTOR IS RESPONSIBLE FOR KEEPING A COPY OF THE APPROVED REPORTS, PLANS, AMENDMENTS, INSPECTION REPORTS, AND PERMITS AT THE CONSTRUCTION SITE AT ALL TIMES UNTIL ALL LAND DISTURBING CONSTRUCTION ACTIVITY IS COMPLETED AND A UNIFORM PERENNIAL VEGETATIVE COVER IS ESTABLISHED WITH A DENSITY OF AT LEAST 70%. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING THE OWNER WHEN THE VEGETATIVE DENSITY REACHES AT LEAST 70%. THE OWNER IS RESPONSIBLE FOR TERMINATING DNR PERMIT COVERAGE.

## AMENDMENTS:

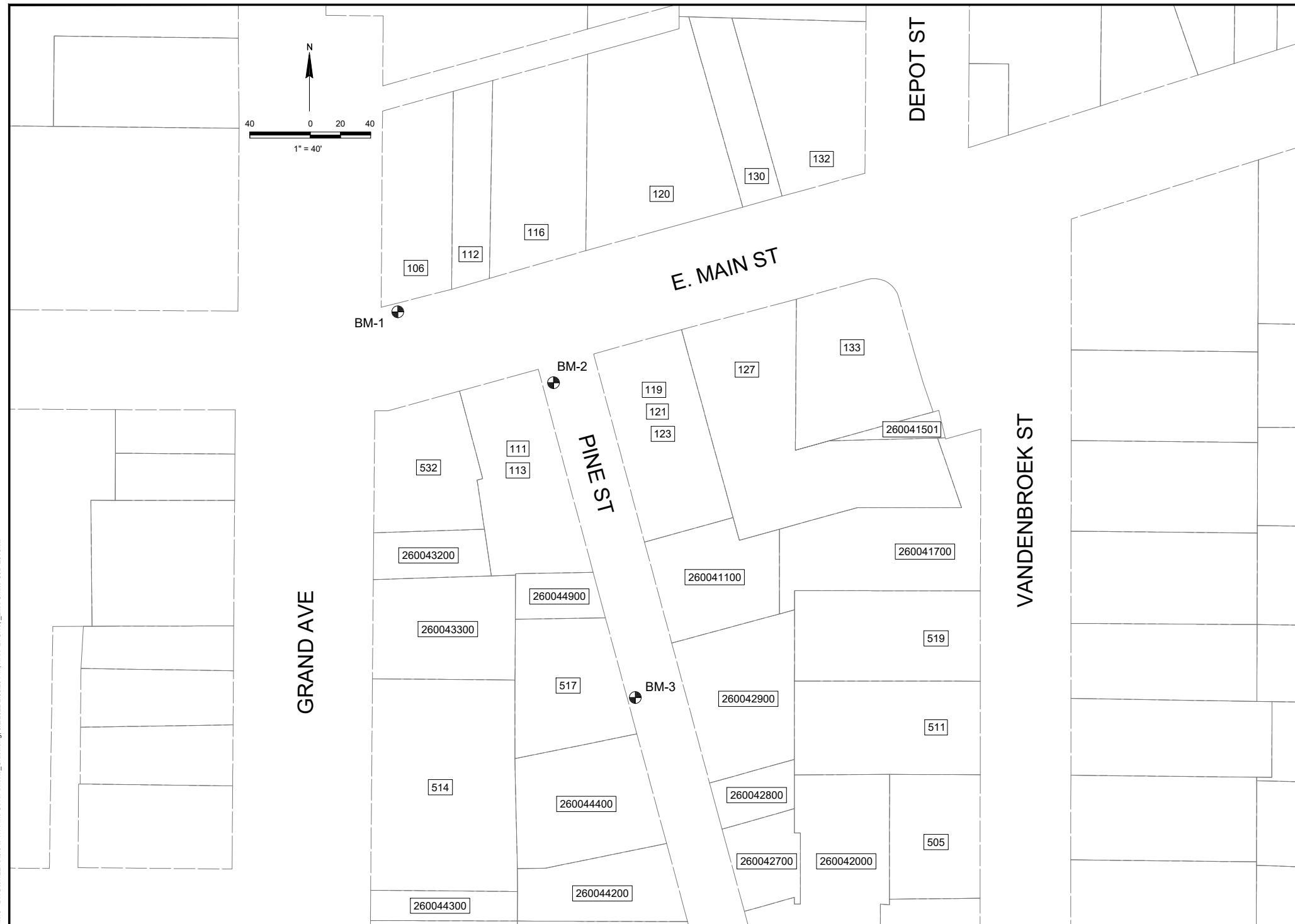
THE CONTRACTOR IS RESPONSIBLE FOR AMENDING THE EROSION & SEDIMENT CONTROL PLAN IF: THERE IS A CHANGE IN CONSTRUCTION, OPERATION OR MAINTENANCE AT THE SITE WHICH HAS THE REASONABLE POTENTIAL FOR THE DISCHARGE OF POLLUTANTS; THE ACTIONS REQUIRED BY THE PLAN FAIL TO REDUCE THE IMPACTS OF POLLUTANTS CARRIED BY CONSTRUCTION RUNOFF; OR IF THE DNR NOTIFIES THE APPLICANT OF CHANGES NEEDED IN THE PLAN. THE DNR AND OWNER SHALL BE NOTIFIED 5 WORKING DAYS PRIOR TO MAKING CHANGES TO THE PLAN.

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

DESIGNED:	
DRAWN:	KAL
CHECKED:	REO
APPROVED:	CLM
PROJECT NUMBER	2022003
SHEET REFERENCE NO.	1.0



2022 PINE STREET RECONSTRUCTION  
ABBREVIATIONS, LEGEND & NOTES  
PINE STREET  
VILLAGE OF LITTLE CHUTE, WISCONSIN



HORIZONTAL DATUM

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), OUTAGAMIE COUNTY.

VERTICAL DATUM

THE VERTICAL DATUM OF THIS SURVEY IS THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), US FOOT.

VERTICAL BENCHMARKS

BM #	ELEVATION	DESCRIPTION
1	739.85	EX HYDRANT TAG BOLT
2	739.60	EX HYDRANT TAG BOLT
3	740.67	PK NAIL

ADDRESS	OWNER NAME	PARCEL ID
514 GRAND AVE	HILST / BERMIT LLP	260043500
	HILST / BERMIT LLP	260044300
532 GRAND AVE	BIRR INVESTMENTS LLC	260043000
106 E MAIN ST	SETHS COFFEE LLC	260071700
111 E MAIN ST	JULBILLIE	260043100
112 E MAIN ST	SAKS HOLDINGS LLC	260071501
116 E MAIN ST	RUSTIC RESQUE LLC	260071500
119 E MAIN ST	S & S REAL ESTATE ENTERPRISES LLC	260041200
120 E MAIN ST	REYNEBEAU, LEE R & LISA A	260071200
127 E MAIN ST	VANDERLOOP REAL ESTATE LLC	260041300
130 E MAIN ST	130 E MAIN LLC	260069300
132 E MAIN ST	MILLS PROPERTY MANAGEMENT IV LLC	260069100
133 E MAIN ST	KERKHOFF PROPERTIES LLC	260041500
	VILLAGE OF LITTLE CHUTE	260043200
	VILLAGE OF LITTLE CHUTE	260043300
	VILLAGE OF LITTLE CHUTE	260044900
517 PINE ST	GNH ENTERPRISES, LLC	260044800
	VILLAGE OF LITTLE CHUTE	260044400
	ST JOHN NEPOMUCENE PARISH	260044200
	VILLAGE OF LITTLE CHUTE	260041100
	VILLAGE OF LITTLE CHUTE	260042900
	VILLAGE OF LITTLE CHUTE	260042800
	ST JOHN NEPOMUCENE PARISH	260042700
	ST JOHN NEPOMUCENE PARISH	260042000
505 VANDENBROEK ST	VANDYNHOVEN APARTMENTS	260042100
511 VANDENBROEK ST	VANGOMPEL, REBECCA J	260041900
519 VANDENBROEK ST	PL219TRIER LLC	260041800
	VILLAGE OF LITTLE CHUTE	260041700
	VANDERLOOP REAL ESTATE LLC	260041501

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

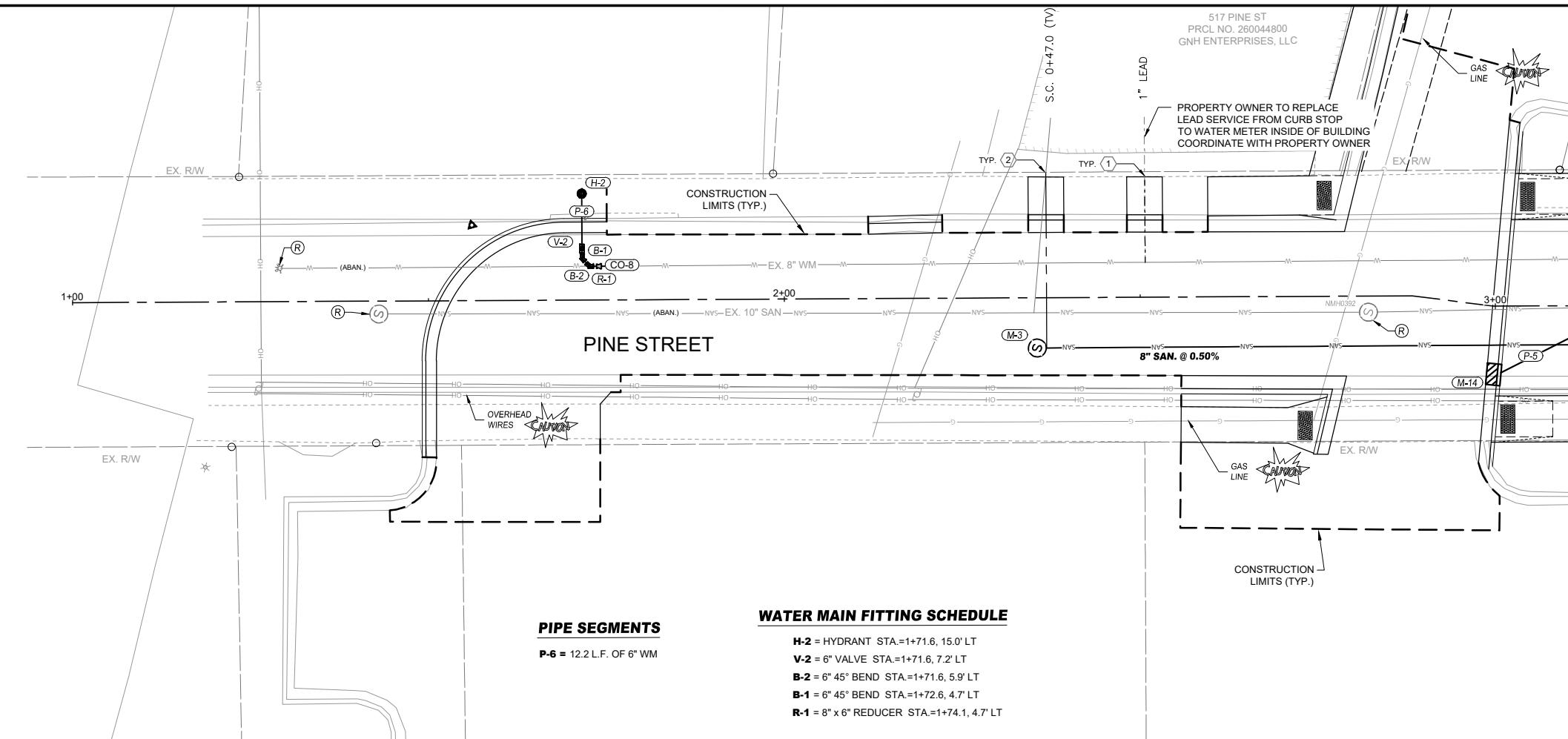
DESIGNED: \_\_\_\_\_  
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CHECKED: REO  
APPROVED: CLM

PROJECT NUMBER  
2022003  
SHEET REFERENCE NO.

1.1

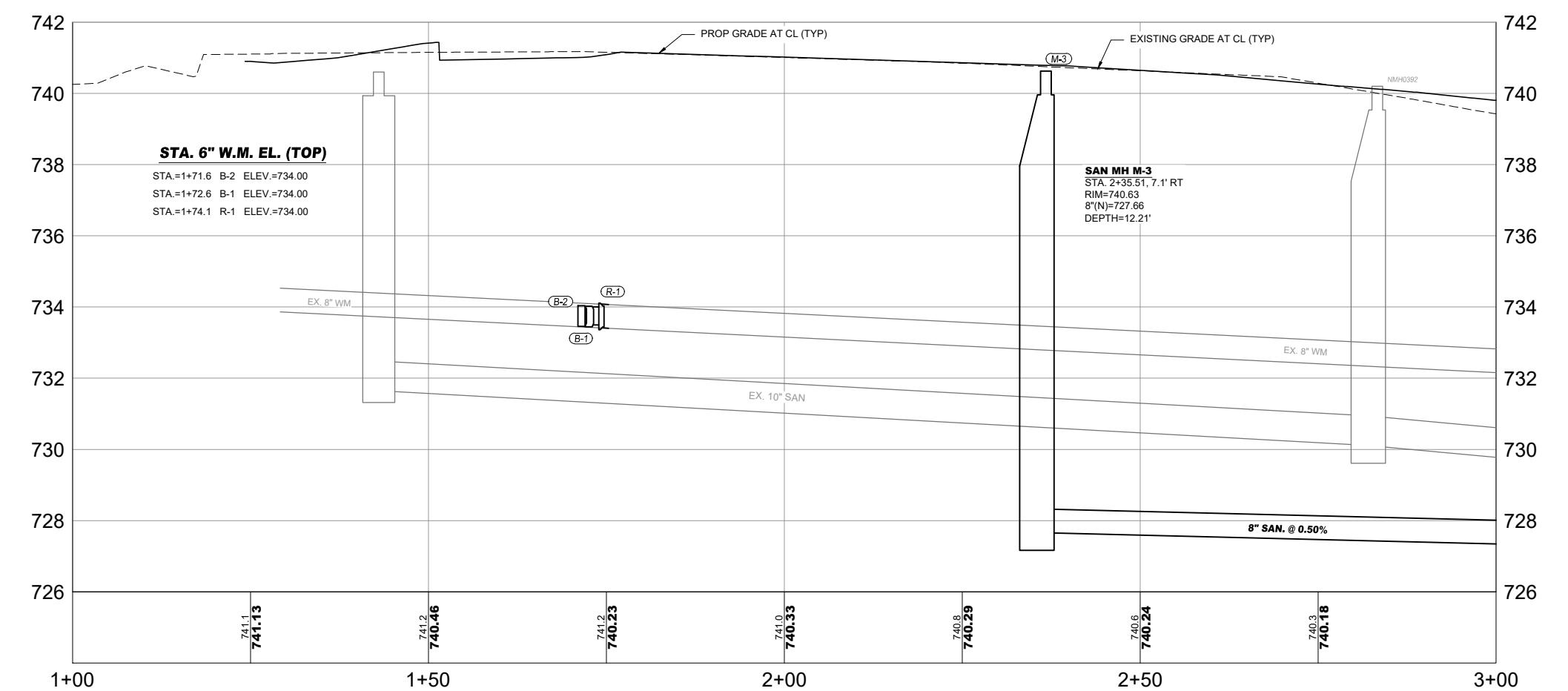
**Little Chute**  
ESTABLISHED 1848  
108 WEST MAIN STREET  
LITTLE CHUTE, WISCONSIN 54140

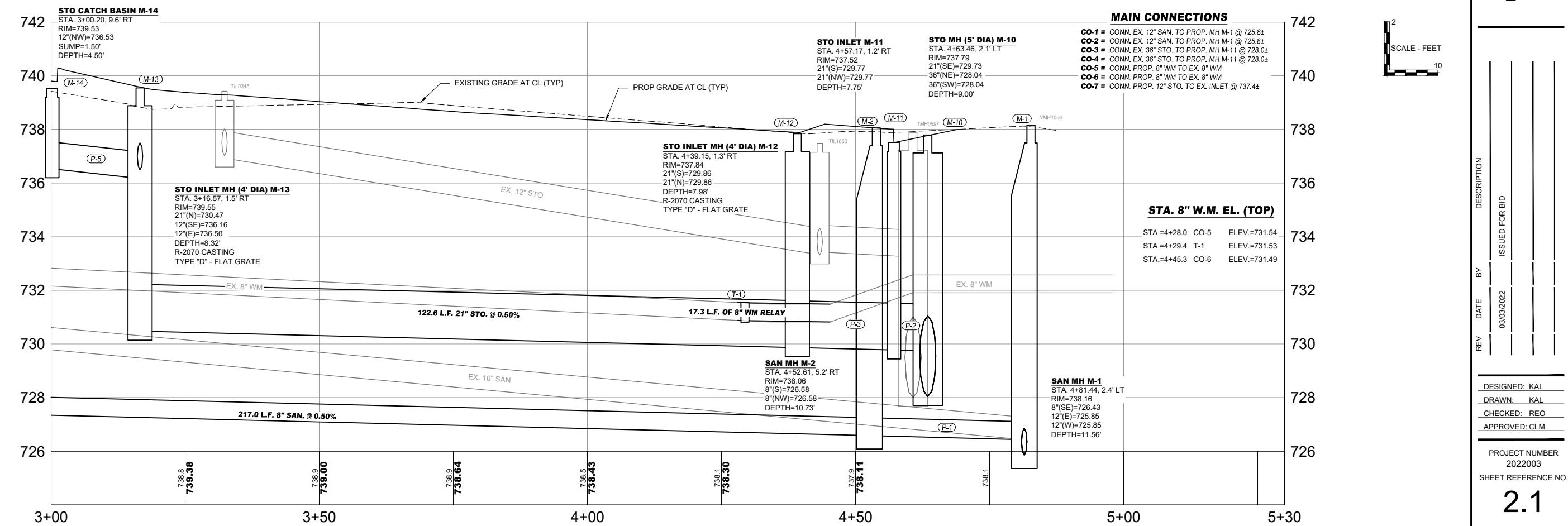
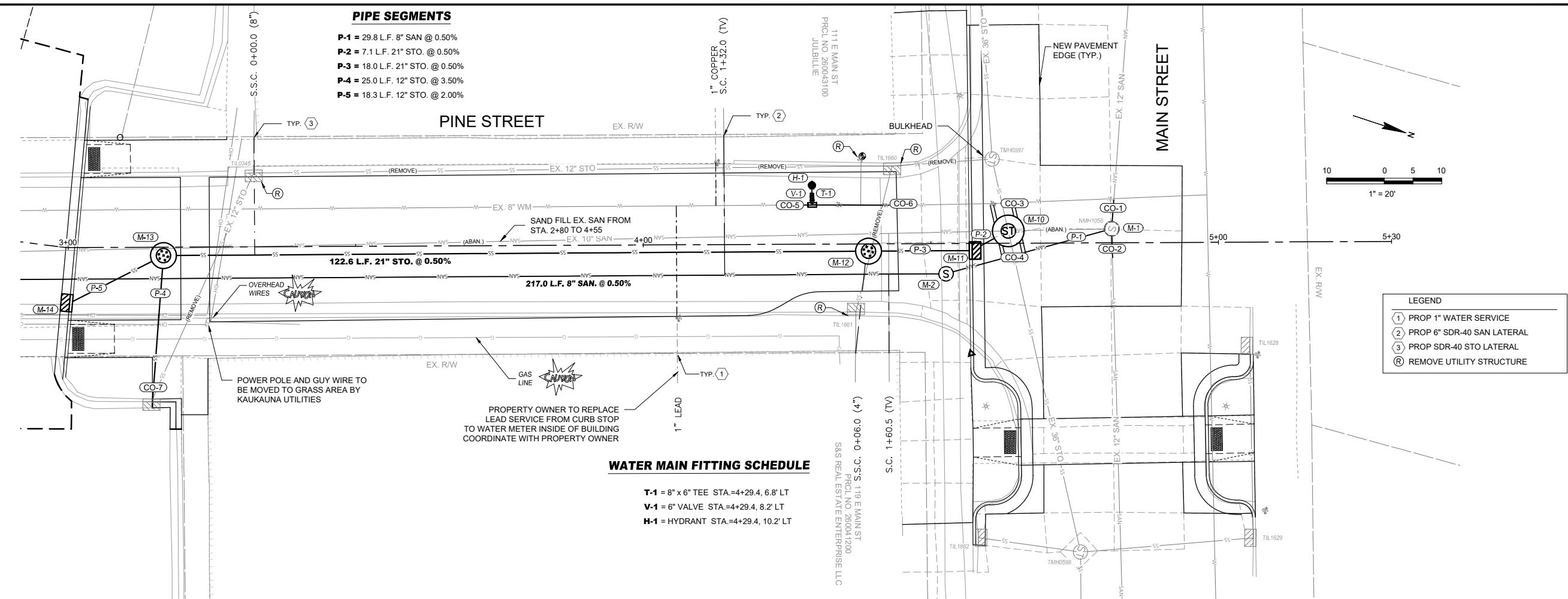
2022 PINE STREET RECONSTRUCTION  
SURVEY CONTROL & PROPERTY OWNER INFO  
PINE STREET  
VILLAGE OF LITTLE CHUTE, WISCONSIN

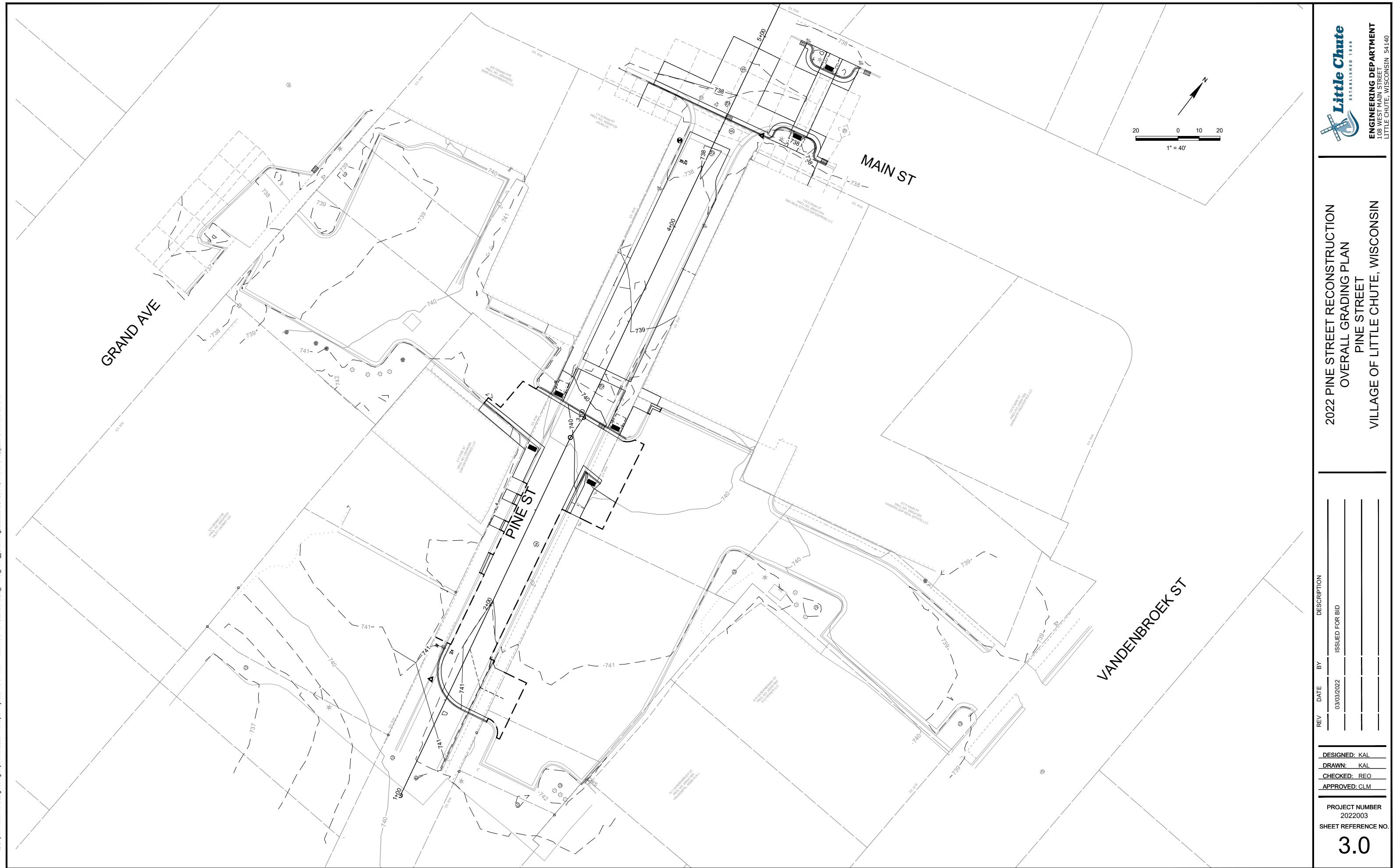


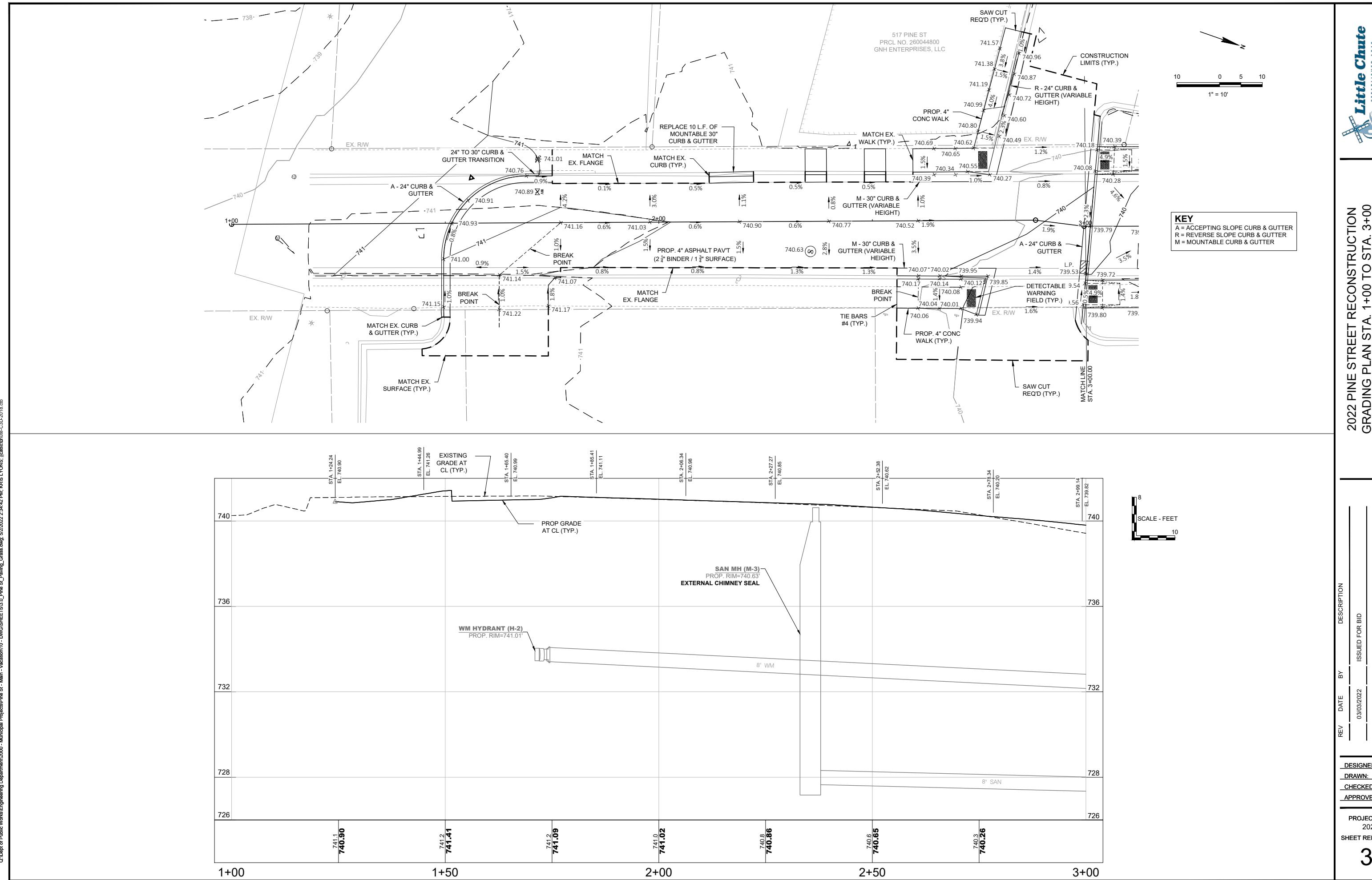
10  
0  
5  
10  
1" = 20'

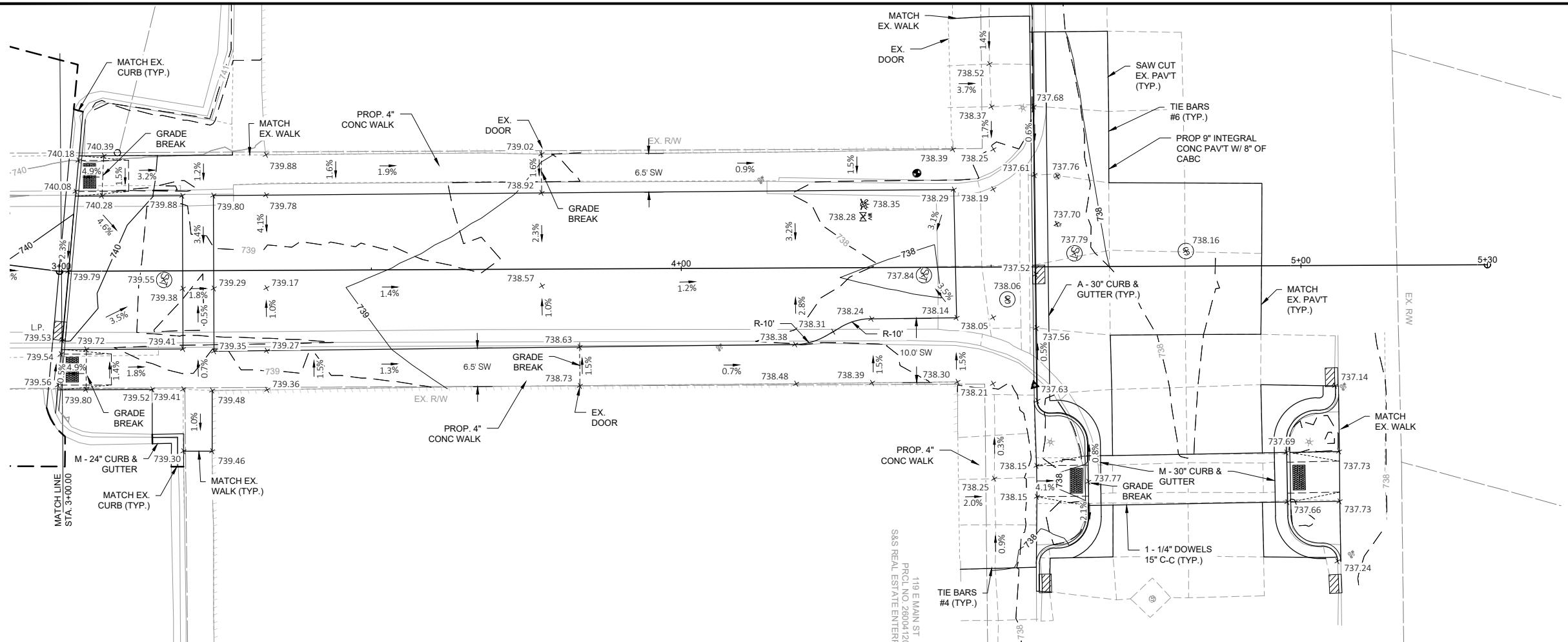
2022 PINE STREET RECONSTRUCTION  
UTILITY PLAN STA. 1+00 TO STA. 3+00  
PINE STREET  
VILLAGE OF LITTLE CHUTE, WISCONSIN





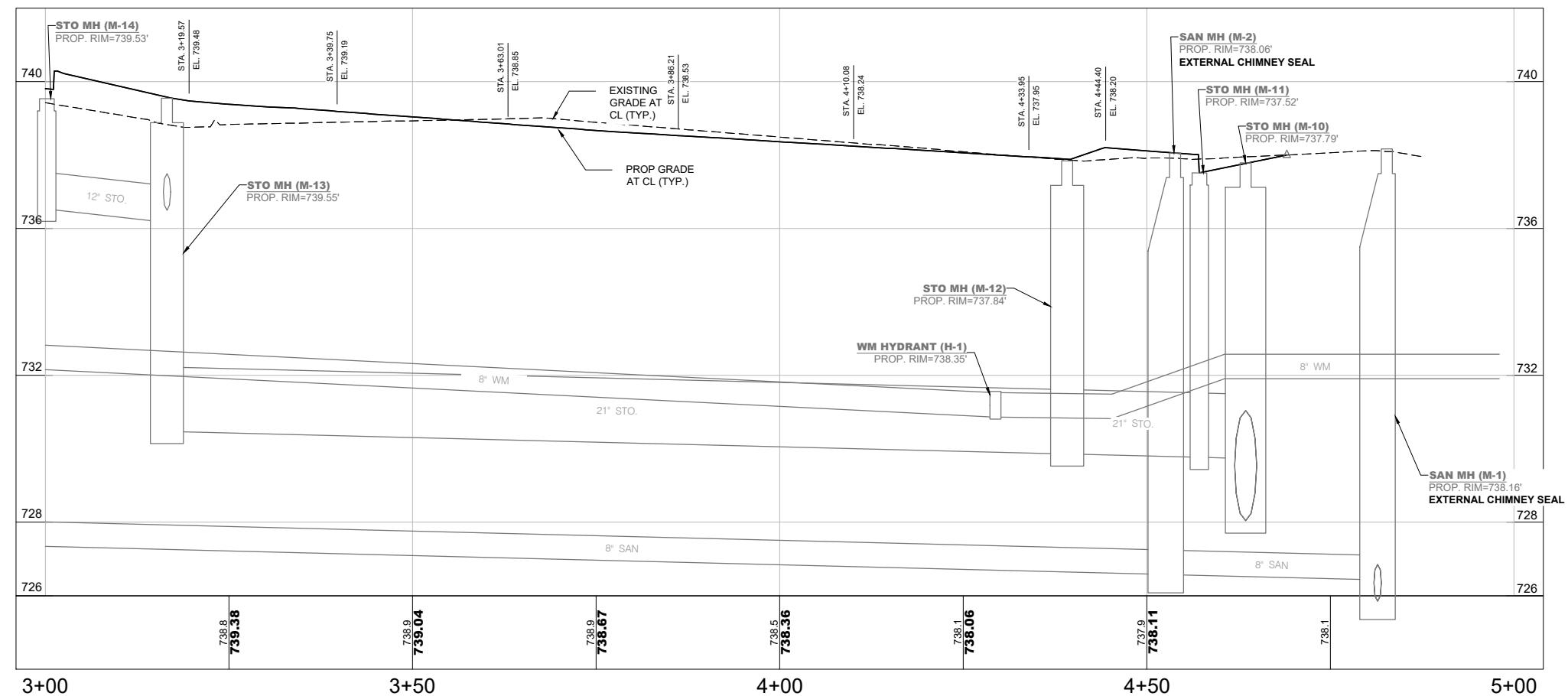






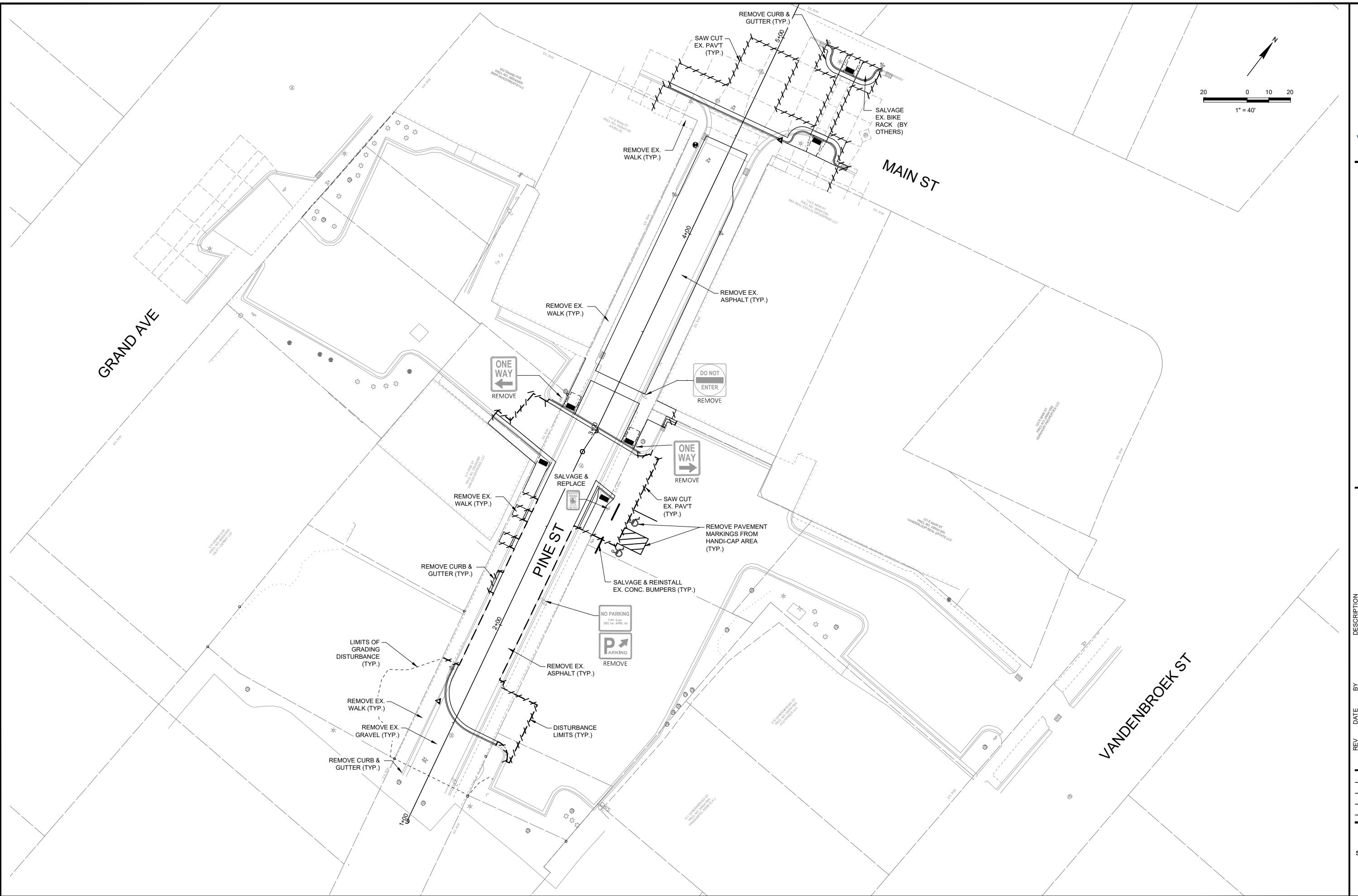
**KEY**  
 A = ACCEPTING SLOPE CURB & GUTTER  
 R = REVERSE SLOPE CURB & GUTTER  
 M = MOUNTABLE CURB & GUTTER

2022 PINE STREET RECONSTRUCTION  
GRADING PLAN STA. 3+00 TO MAIN STREET  
PINE STREET  
VILLAGE OF LITTLE CHUTE, WISCONSIN

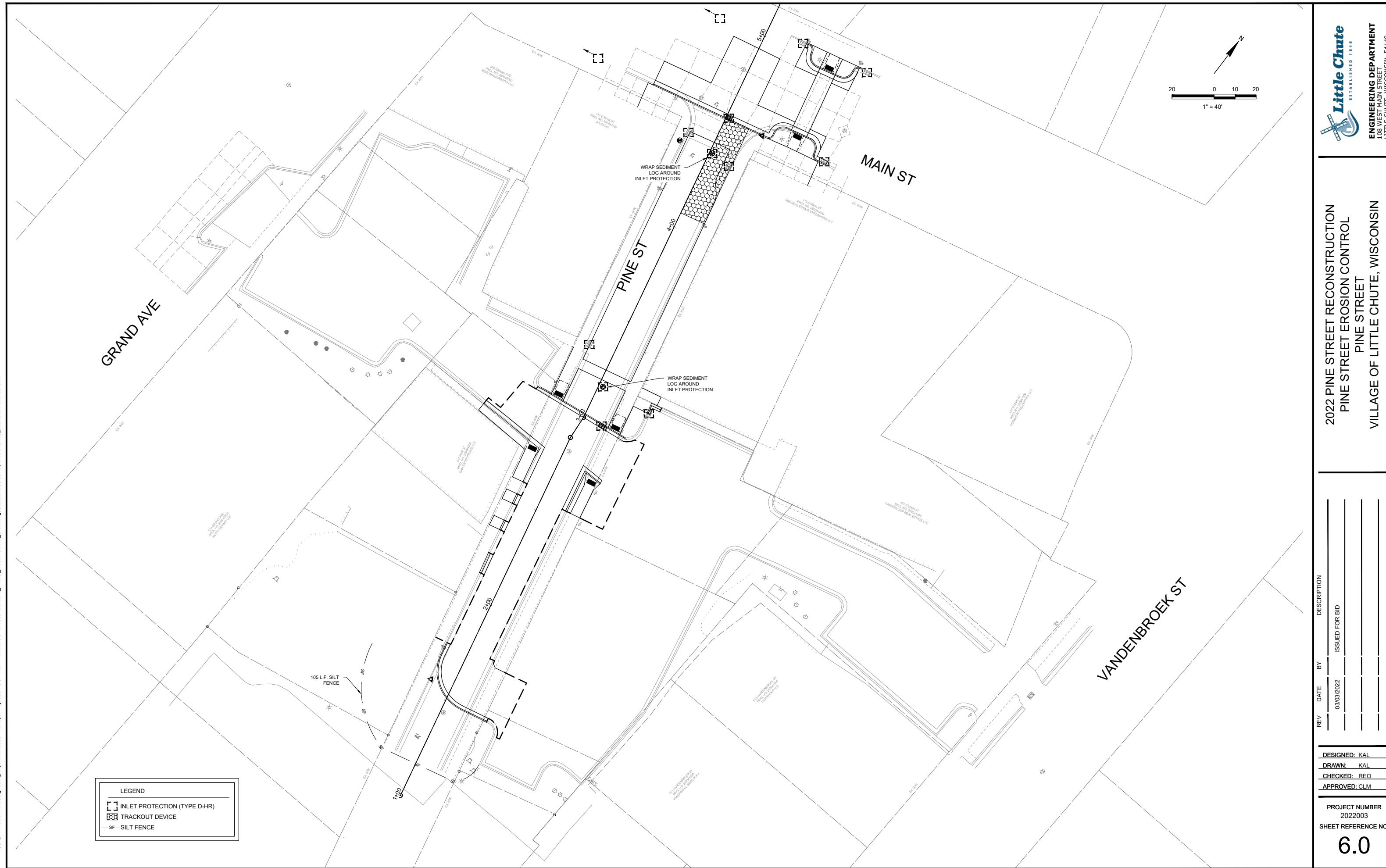


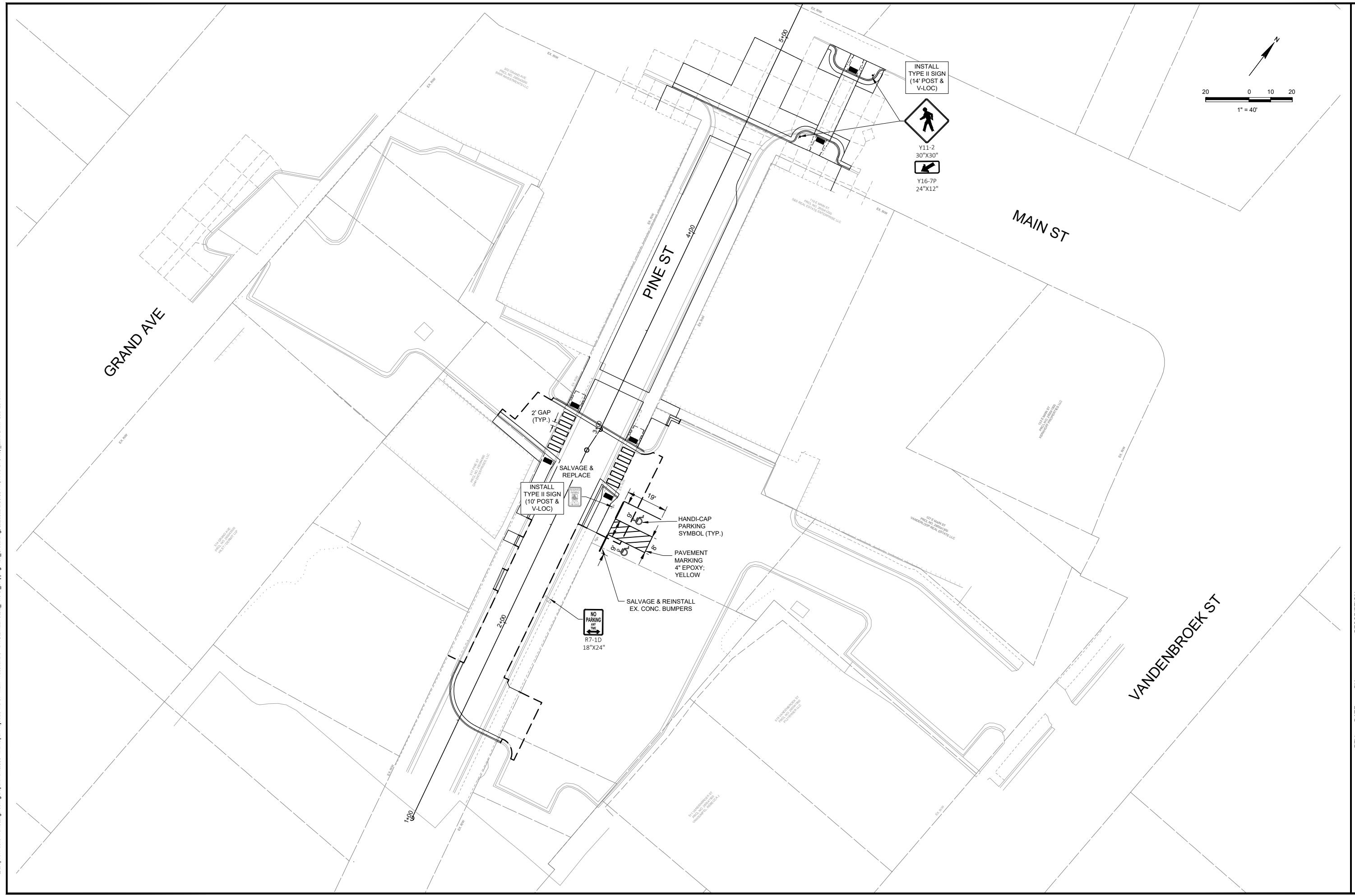
REV. DATE BY DESCRIPTION  
03/03/2022 ISSUED FOR BID

DESIGNED: KAL  
DRAWN: KAL  
CHECKED: REO  
APPROVED: CLM  
PROJECT NUMBER 2022003  
SHEET REFERENCE NO. 3.2









**SEQUENCE OF CONSTRUCTION**  
THE CONTRACTOR SHALL COMPLY WITH THE FOLLOWING SEQUENCE AND SCHEDULE OF MAJOR LAND DISTURBING ACTIVITIES:  
A. POST PERMITS AT SITE, NOTIFY VILLAGE OF LITTLE CHUTE AND ENGINEER AT LEAST 5 DAYS PRIOR TO THE START OF UTILITY CONSTRUCTION (PHASE 1).  
B. INSTALL INLET PROTECTION BEFORE BEGINNING ANY LAND DISTURBING CONSTRUCTION ACTIVITIES. INSTALL TRACK-OUT CONTROL PRACTICE, IF NEEDED. STREET SWEEPING SHALL BE PERFORMED ON A DAILY BASIS OR AS NEEDED.  
C. SAW CUT AND REMOVE PAVEMENT AS NEEDED FOR UTILITY REPLACEMENT. INSTALL SANITARY SEWER, STORM SEWER, AND WATER MAIN. INSTALL INLET PROTECTION FOR NEW STORM INLETS IMMEDIATELY AFTER THE STORM INLET IS INSTALLED. DE-WATER TRENCHES AS NEEDED. EXCESS EXCAVATED MATERIAL SHALL BE LOADED ON TRUCKS AND HAULED TO AN OFFSITE LOCATION SELECTED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR PERMITS AND BEST MANAGEMENT PRACTICES AT THE OFFSITE LOCATION. TEMPORARILY STABILIZE DISTURBED AREAS LOCATED IN EXISTING PAVEMENT AREAS WITH A GRAVEL/PAVEMENT SURFACE, SUCH THAT NO MORE THAN 200 LINEAR FEET OF UTILITY TRENCH LENGTH IN EXISTING PAVEMENT AREAS IS EXPOSED SOIL AT THE END OF EACH DAY.  
D. THE UTILITY CONTRACTOR IS RESPONSIBLE FOR TEMPORARY STABILIZATION PRACTICES AND INSTALLING, REPAIRING, MAINTAINING, AND INSPECTING EROSION AND SEDIMENT CONTROLS UNTIL THE STREET CONTRACTOR BEGINS WORK ALONG EACH STREET SEGMENT.  
E. POST PERMITS AT SITE, NOTIFY VILLAGE OF LITTLE CHUTE AND ENGINEER AT LEAST 5 DAYS PRIOR TO THE START OF STREET CONSTRUCTION (PHASE 2).  
F. MAINTAIN EROSION AND SEDIMENT CONTROL PRACTICES PREVIOUSLY INSTALLED BY THE UTILITY CONTRACTOR (PHASE 1).  
G. EXCAVATE THE STREET. EXCESS EXCAVATED MATERIAL SHALL BE LOADED ON TRUCKS AND HAULED TO AN OFFSITE LOCATION SELECTED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR PERMITS AND BEST MANAGEMENT PRACTICES AT THE OFFSITE LOCATION. IN NEW PAVEMENT AREAS, STABILIZE EXPOSED SOIL WITH GRAVEL BASE. NO MORE THAN 200 LINEAR FEET OF STREET SUB-GRADE SHALL BE EXPOSED SOIL AT THE END OF EACH DAY, INCLUDING DRIVEWAYS LOCATED ALONG THE 200 LINEAR FEET STREET LENGTH.  
H. INSTALL CONCRETE AND ASPHALT PAVEMENTS. PLACE TOPSOIL IN LAWN AREAS. STABILIZE LAWN AREAS WITHIN 14 DAYS OF TOPSOIL PLACEMENT. RESTORE LAWN AREAS WITH TOPSOIL, LAWN SEED, FERTILIZER AND MULCH.

**A CONSTRUCTION SITE SEQUENCING**  
8.0 NOT TO SCALE

**WDNR - CONSERVATION PRACTICE STANDARD - TRACKOUT CONTROL PRACTICES:**  
CONTRACTOR SHALL PROVIDE THE STONE TRACKING PAD IN ACCORDANCE WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES CONSERVATION STANDARD No. 1057.

TRACKING PAD SHALL BE INSTALLED PRIOR TO ANY TRAFFIC LEAVING THE SITE. IF CONDITIONS ARE SUCH THAT SEDIMENT IS NOT REMOVED FROM TIRES BY THE TRACKING PAD, THEN TIRES SHALL BE WASHED IN ACCORDANCE WITH WDNR TECHNICAL STANDARD No. 1057.

INSTALL THE TRACKING PAD ACROSS THE FULL WIDTH OF THE ACCESS POINT, OR RESTRICT EXITING TRAFFIC TO A DEDICATED EGRESS LANE WITH A DRIVING SURFACE AT LEAST 12 FEET WIDE.

ENSURE THE TRACKING PAD IS AT LEAST 50 FEET LONG. IF A 50-FOOT PAD LENGTH IS NOT POSSIBLE DUE TO SITE CONSTRAINTS, INSTALL THE MAXIMUM LENGTH PRACTICABLE AND SUPPLEMENT WITH ADDITIONAL PRACTICES AS NEEDED TO PREVENT OR REDUCE TRACKOUT.

WHERE WARRANTED DUE TO SOIL TYPE OR HIGH GROUNDWATER, UNDERLAY THE STONE TRACKING PAD WITH GEOTEXTILE FABRIC TO MINIMIZE MIGRATION OF UNDERLYING SOIL INTO THE STONE. SELECT FABRIC TYPE BASED ON SOIL CONDITIONS AND VEHICLE LOADING.

PLACE THE AGGREGATE IN A LAYER AT LEAST 12 INCHES THICK.

DIVERT SURFACE FLOWS AWAY FROM TRACKING PADS OR CONVEY FLOW UNDER AND/OR AROUND USING CULVERTS AND SWALES. DIRECT RUNOFF FROM TRACKING PADS TO SEDIMENT CONTROL PRACTICES.

DO NOT COMPACT AGGREGATE PRIOR TO USE. COMPACTION, GROUTING, OR OTHER MEANS OF CREATING A SMOOTH SURFACE COMPROMISE THE EFFECTIVENESS OF THE TRACKING PAD.

REMOVE STONES LODGED BETWEEN THE TIRES OF DUAL WHEEL VEHICLES PRIOR TO LEAVING THE CONSTRUCTION SITE.

MONITOR TRACKING PADS FOR COMPACTION, SOIL DEPOSITS, AND MIXING OF UNDERLYING SOILS AND STONE LAYERS.

MAINTAIN A LOOSENED, ROUGH SURFACE BY SCRAPING, LOOSENING, OR TOP-DRESSING WITH ADDITIONAL AGGREGATE.

REPLACE GEOTEXTILE AND STONE IF LESS-INTENSIVE MAINTENANCE EFFORTS FAIL TO RE-ESTABLISH EFFECTIVENESS.

ADD STONE AS NEEDED TO MAINTAIN THE MINIMUM PAD THICKNESS.

REPLACE DAMAGED OR CRUSHED CULVERTS UNDER TRACKING PAD.

① USE HARD, DURABLE, ANGULAR STONE OR RECYCLED CONCRETE MEETING THE GRADATION IN TABLE 1. WHERE THIS GRADATION IS NOT AVAILABLE, MEET THE GRADATION IN WISCONSIN DEPARTMENT OF TRANSPORTATION (DOT) 2018 STANDARD SPECIFICATION, SECTION 312, SELECT CRUSHED MATERIAL.

② SLOPE THE STONE TRACKING PAD IN A MANNER TO DIRECT RUNOFF TO AN APPROVED TREATMENT PRACTICE.

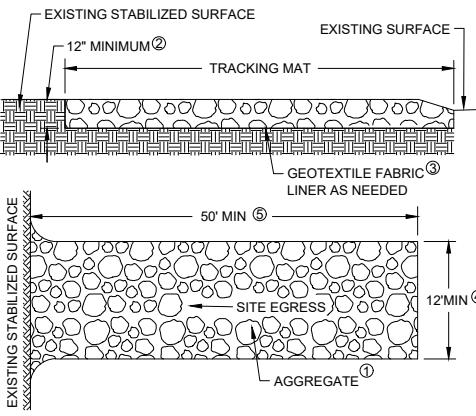
③ SELECT FABRIC TYPE BASED ON SOIL CONDITIONS AND VEHICLE LOADING.

④ INSTALL TRACKING PAD ACROSS FULL WIDTH OF THE ACCESS POINT, OR RESTRICT EXISTING TRAFFIC TO A DEDICATED EGRESS LANE AT LEAST 12 FEET WIDE ACROSS THE TOP OF THE PAD.

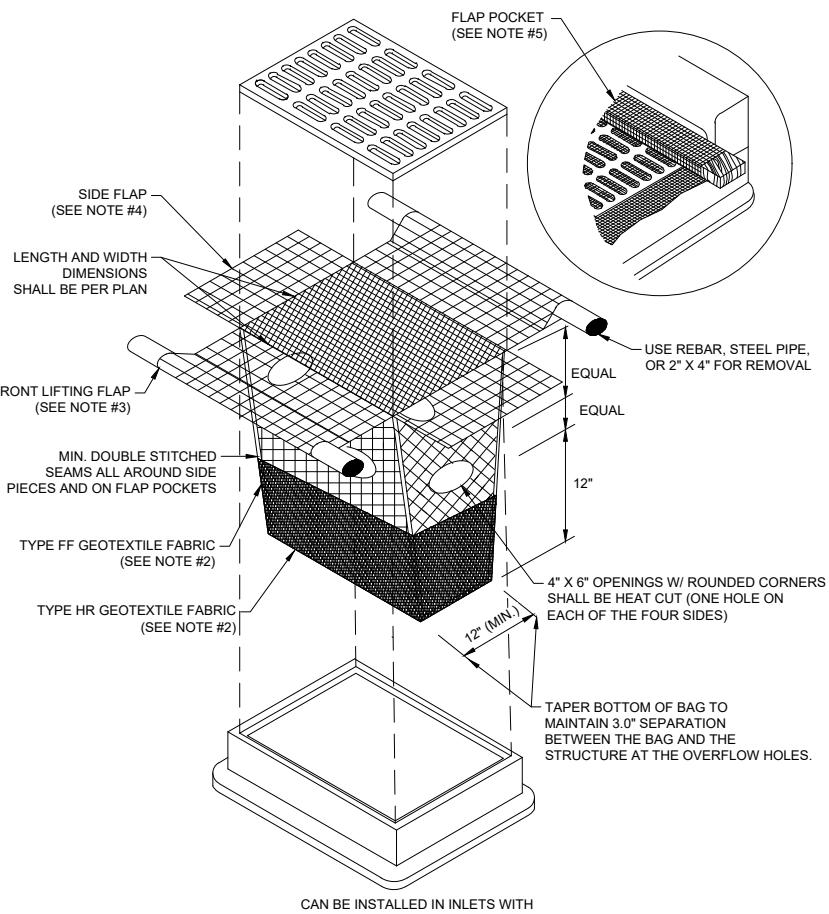
⑤ IF A 50' PAD LENGTH IS NOT POSSIBLE DUE TO SITE GEOMETRY, INSTALL THE MAXIMUM LENGTH PRACTICABLE AND SUPPLEMENT WITH ADDITIONAL PRACTICES AS NEEDED.

⑥ SWEEP SEDIMENT FROM ROAD SURFACE DAILY OR UPON TRACKING OF MATERIAL ONTO ANY PAVED SURFACES TO PREVENT ANY MATERIAL FROM LEAVING THE WORKING AREA.

TABLE 1 GRADATION FOR STONE TRACKING PADS	
SIEVE SIZE	PERCENT BY WEIGHT PASSING
3"	100
2-1/2"	90-100
1-1/2"	25-60
3/4"	0-20
3/8"	0-5



**B TRACKOUT CONTROL**  
8.0 NOT TO SCALE



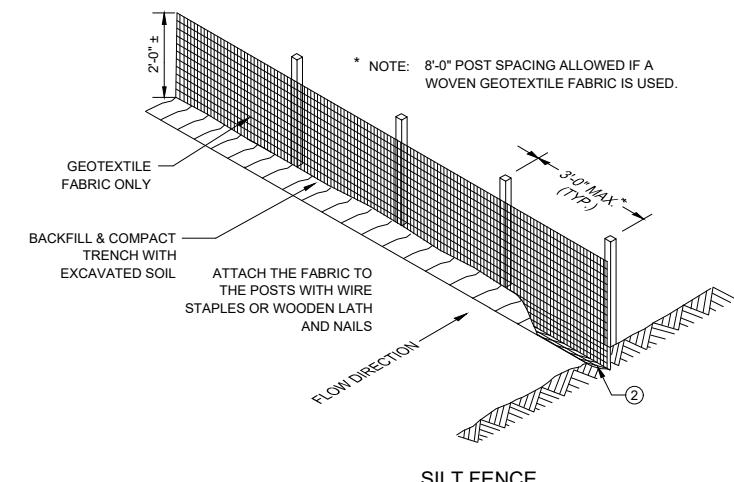
**GENERAL NOTES**

1. TAPER BOTTOM OF BAG TO MAINTAIN THREE INCHES OF CLEARANCE BETWEEN THE BAG AND THE STRUCTURE, MEASURED FROM THE BOTTOM OF THE OVERFLOW OPENING TO THE STRUCTURE WALL.
2. GEOTEXTILE FABRIC TYPE FF FOR FLAPS, TOP HALF OF FILTER BAG, GEOTEXTILE FABRIC, TYPE HR FOR BOTTOM HALF OF FILTER BAG WITH FRONT, BACK, AND BOTTOM BEING ONE PIECE.
3. FRONT LIFTING FLAP IS TO BE USED WHEN REMOVING AND MAINTAINING FILTER BAG.
4. SIDE FLAPS SHALL BE A MAXIMUM OF TWO INCHES LONG. FOLD THE FABRIC OVER AND REINFORCE WITH MULTIPLE STITCHES.
5. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2x4", THE REBAR, STEEL PIPE, OR WOOD SHALL BE INSTALLED IN THE REAR FLAP AND SHALL NOT BLOCK THE TOP HALF OF THE CURB FACE OPENING.

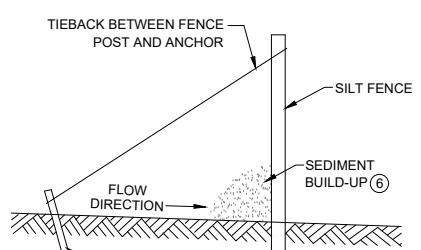
**INSTALLATION NOTES**  
(TYPE D) DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



THIS DRAWING BASED ON WISCONSIN DEPARTMENT OF TRANSPORTATION STANDARD DETAIL DRAWING 8 E 9-6.

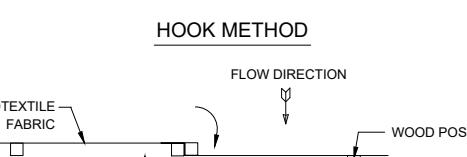
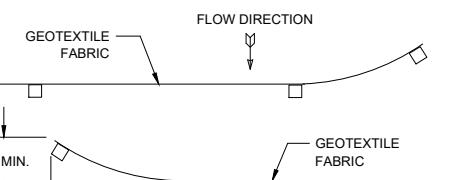


**(WHEN ADDITIONAL SUPPORT REQUIRED)**

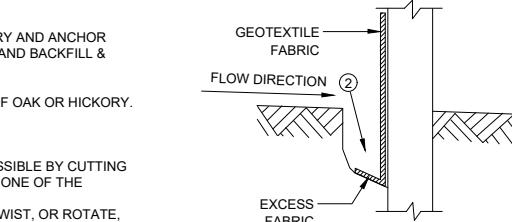
**GENERAL NOTES**

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1-1/8" X 1-1/8" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS:
  - A) TWIST METHOD - OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES;
  - B) HOOK METHOD - HOOK THE END OF EACH SILT FENCE LENGTH.
6. REMOVE SEDIMENT BEHIND SILT FENCE WHEN REACHING 1/2 THE HEIGHT.

**JOINING TWO LENGTHS OF SILT FENCE** ⑤

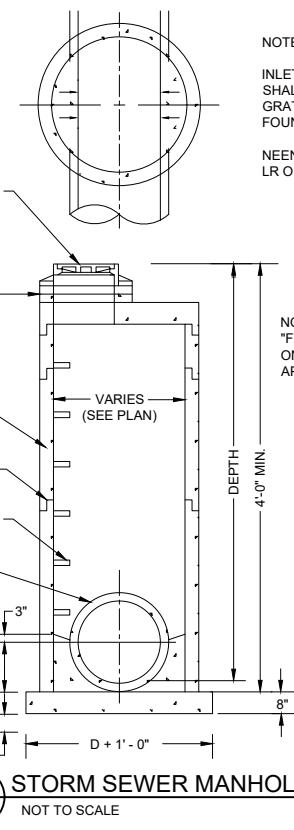


**TWIST METHOD**

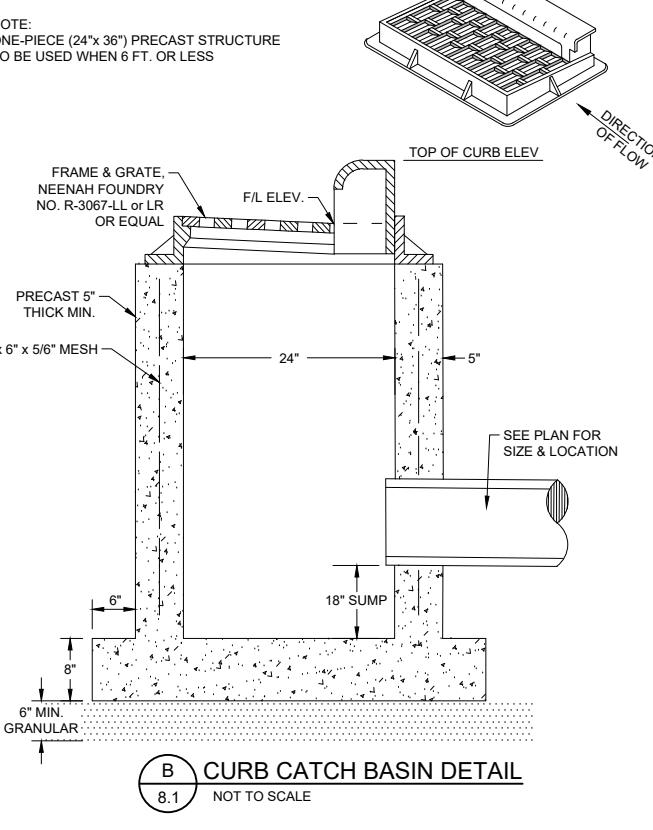


**C INLET PROTECTION TYPE D-HR**  
8.0 NOT TO SCALE

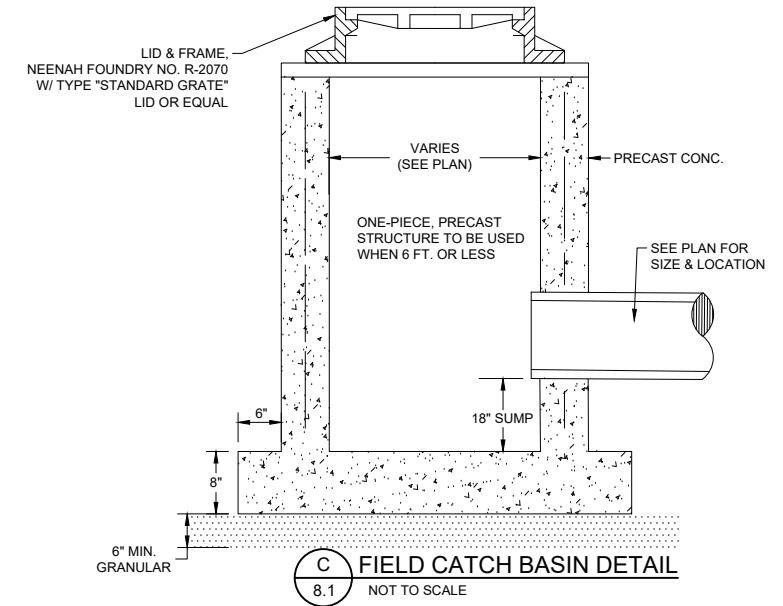
**D SILT FENCING DETAILS**  
8.0 NOT TO SCALE



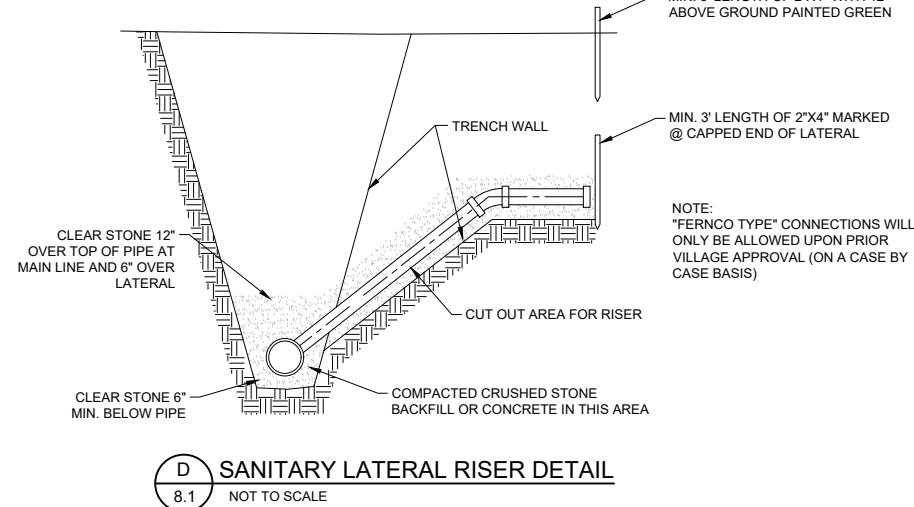
NOTE:  
INLET MANHOLE (MH) STRUCTURES  
SHALL HAVE THE FOLLOWING FRAME &  
GRATE IN LIEU OF THE NEENAH  
FOUNDRY NO R-1550 LID AND FRAME:  
NEENAH FOUNDRY NO. R-3067-LL OR  
LR OR EQUAL.



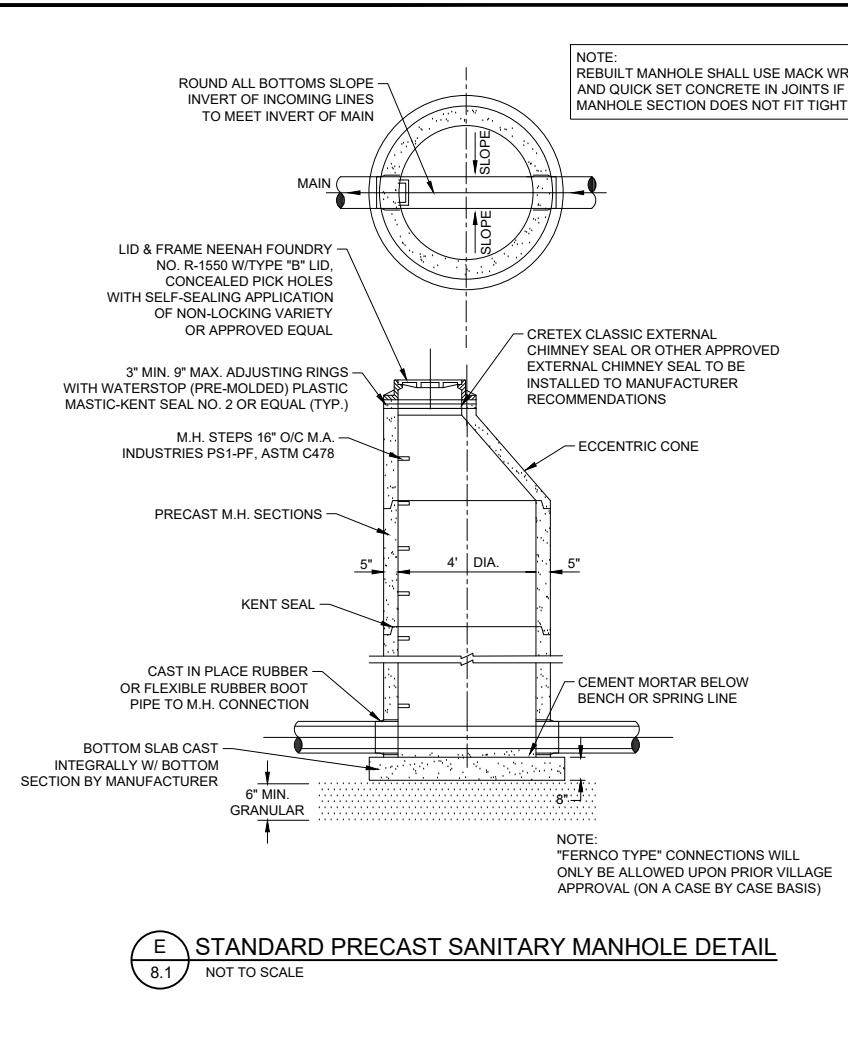
NOTE:  
ONE-PIECE (24"x 36") PRECAST STRUCTURE  
TO BE USED WHEN 6 FT. OR LESS



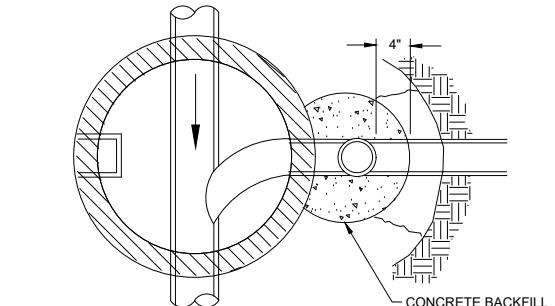
**C FIELD CATCH BASIN DETAIL**  
8.1 NOT TO SCALE



**D SANITARY LATERAL RISER DETAIL**  
8.1 NOT TO SCALE



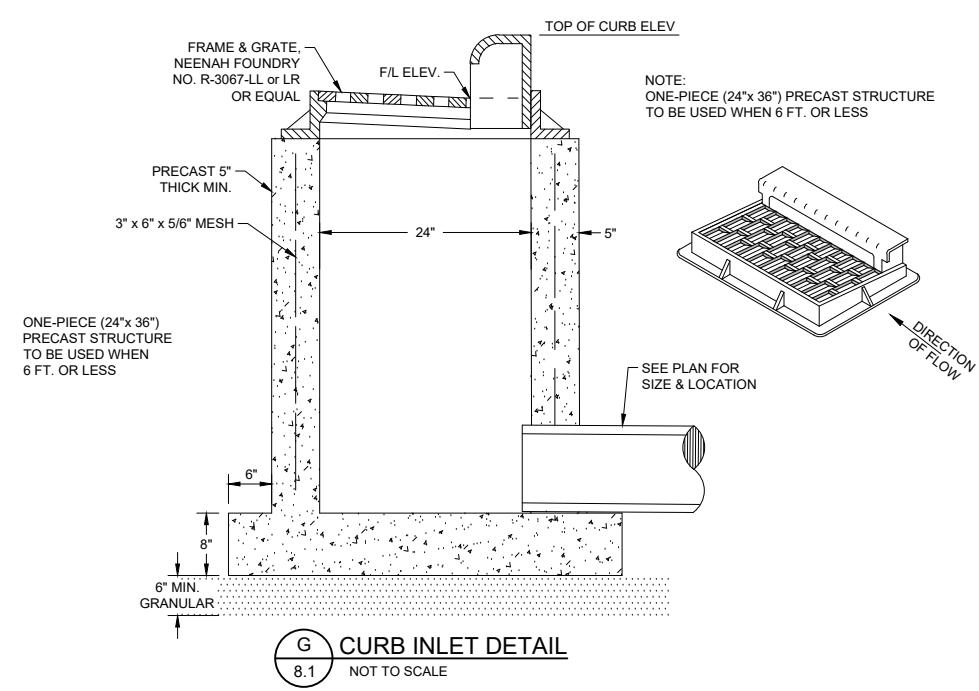
NOTE:  
INLET MANHOLE (MH) STRUCTURES  
SHALL HAVE THE FOLLOWING FRAME &  
GRATE IN LIEU OF THE NEENAH FOUNDRY NO R-1550 LID AND FRAME:  
NEENAH FOUNDRY NO. R-3067-LL OR LR OR EQUAL.



WHENEVER "X" IS  
GREATER THAN 2 FT, AN  
OUTSIDE DROP MUST BE  
CONSTRUCTED AS SHOWN.

NOTE:  
F/L OF DROP TO BE SET  
AT SAME ELEVATION AS  
SPRING LINE OF MAIN

**F OUTSIDE DROP PRECAST MANHOLE**  
8.1 NOT TO SCALE



**G CURB INLET DETAIL**  
8.1 NOT TO SCALE

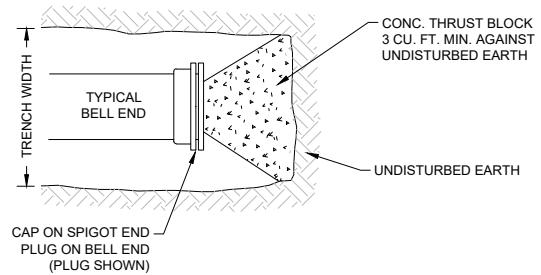
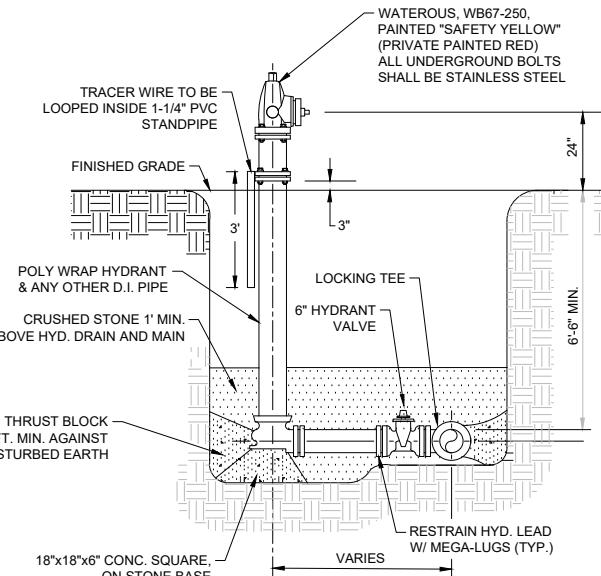
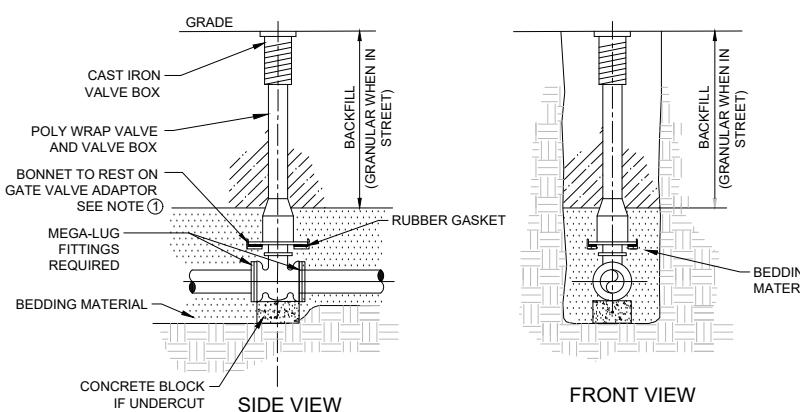
MINIMUM RESTRAINT LENGTH (FT) ON BOTH SIDES OF THE FITTING				
FITTING TYPE/NOMINAL SIZE	6"	8"	12"	16"
11 1/4° BEND	2	2	3	3
22 1/2° BEND	3	3	5	6
45° BEND	5	6	9	11
90° BEND	11	15	21	27
DEAD END	30	40	56	73
TOP SIDE OF A VERTICAL OFFSET	13	17	24	31
TEE RUN X BRANCH 6"BY	14			
TEE RUN X BRANCH 8"BY	10	24		
TEE RUN X BRANCH 12"BY	1	15	40	
TEE RUN X BRANCH 16"BY	1	7	33	56

## NOTES:

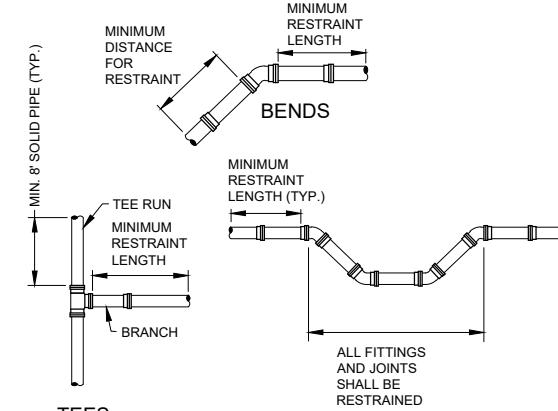
1. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 P.S.I. AND ON AN EARTH RESISTANCE OF 2 TONS PER SQUARE FOOT.

## GENERAL NOTE

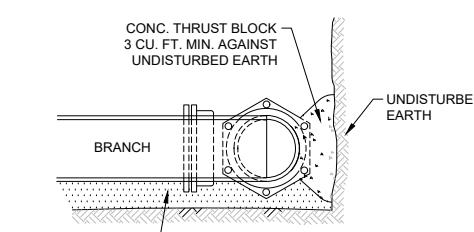
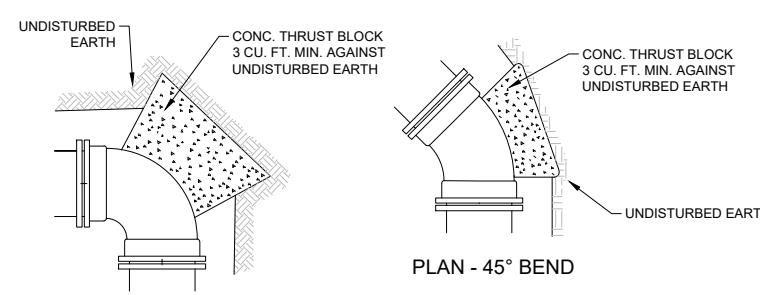
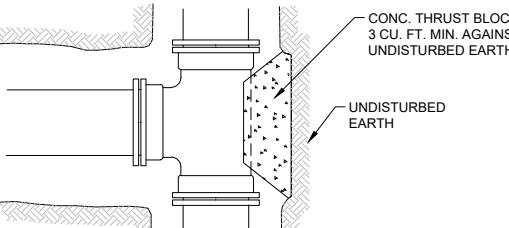
① GATE VALVE ADAPTOR TO BE MADE OUT OF 1/4" STEEL, WITH UV POLYURETHANE PROTECTIVE COATING, AND HAVE 3/4" RUBBER GASKET ATTACHED TO THE GATE VALVE ADAPTOR.



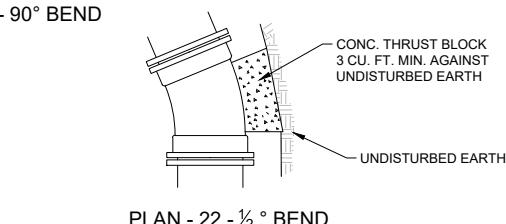
**A BLOCKING FOR PLUGS**  
8.2 NOT TO SCALE



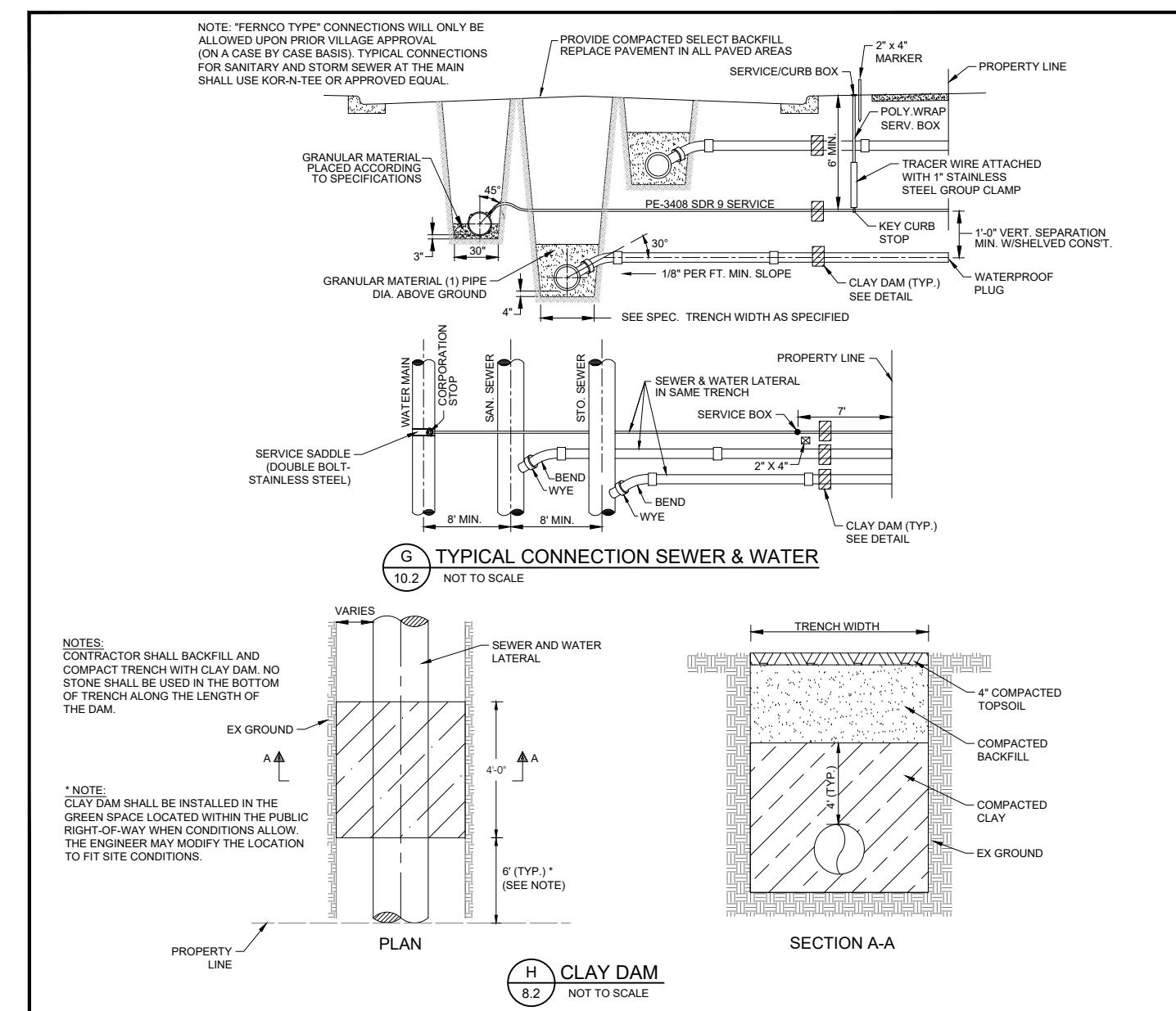
**D WATER MAIN RESTRAINT DETAIL**  
8.2 NOT TO SCALE



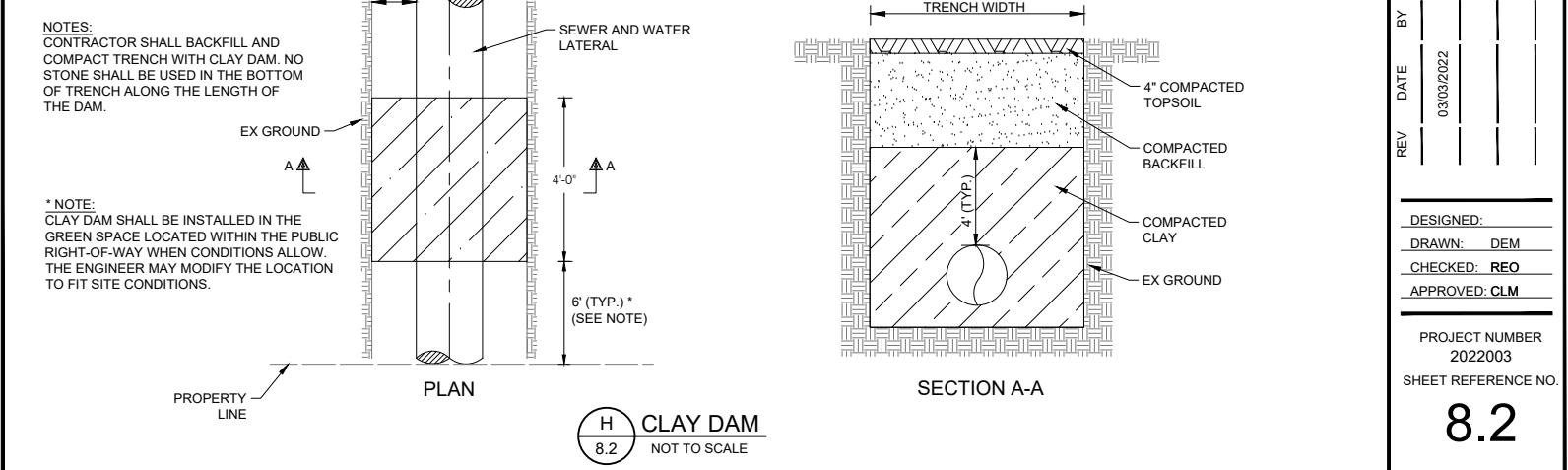
**E BLOCKING FOR TEES**  
8.2 NOT TO SCALE



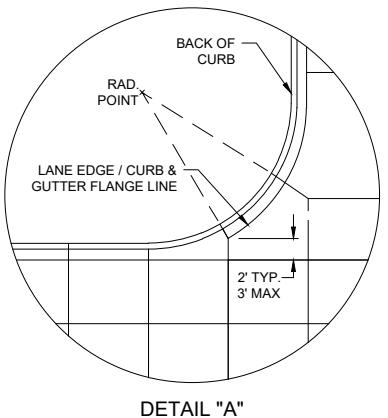
**F BLOCKING FOR WATER MAIN BENDS**  
8.2 NOT TO SCALE



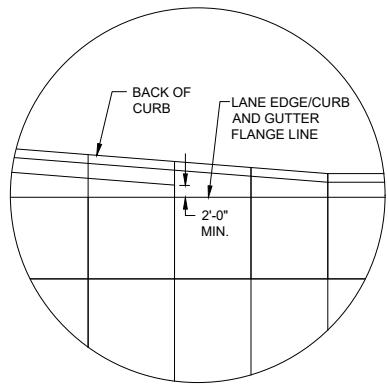
**G TYPICAL CONNECTION SEWER & WATER**  
10.2 NOT TO SCALE



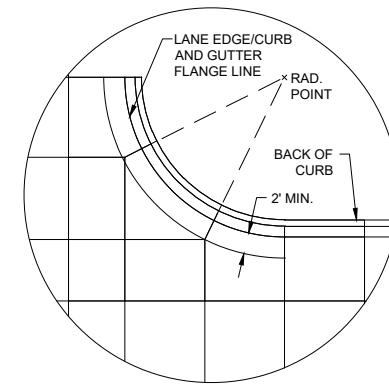




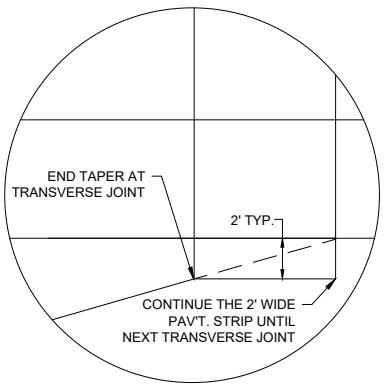
DETAIL "A"



DETAIL "B"



DETAIL "C"

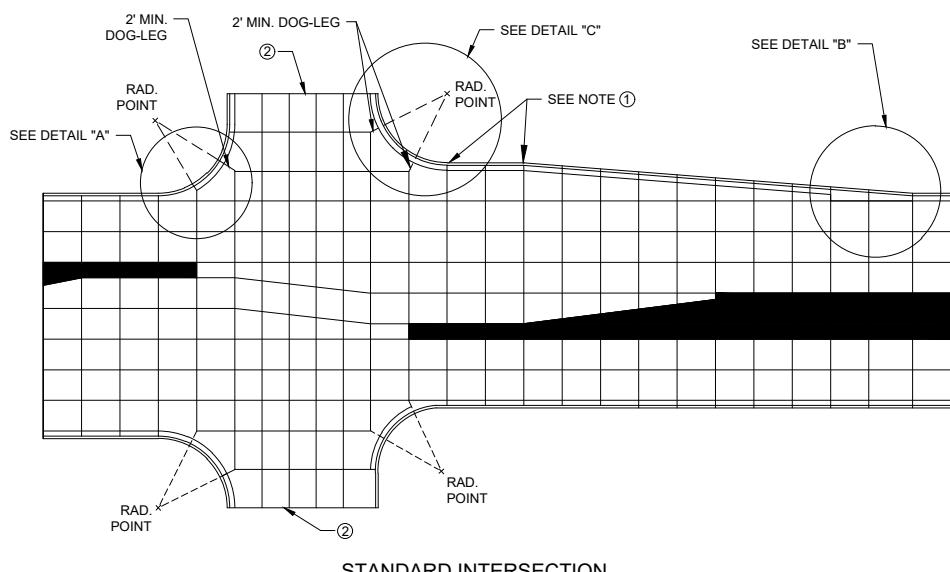


DETAIL "D"

#### GENERAL NOTES

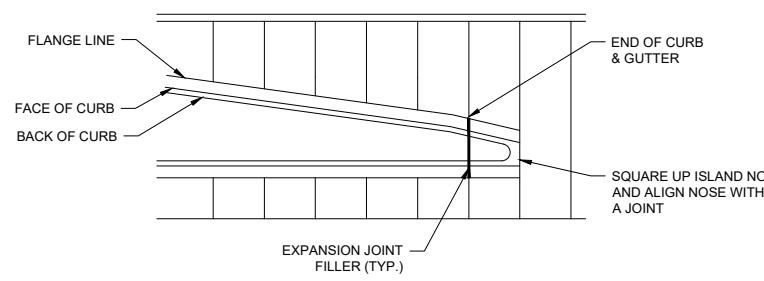
THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN.  
 ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.  
 CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.  
 ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G. MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.  
 AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.  
 SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.  
 AVOID ANGLES LESS THAN 60° BY DOG-LEGGING JOINTS THROUGH CURVE RADIUS POINTS; USE 90° ANGLES WHEN POSSIBLE.  
 CORRELATE LONGITUDINAL JOINTS WITH LANES WHEN POSSIBLE.

1. PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.
2. CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300' IN LENGTH, ALIGN EXPANSION JOINT WITH EDGE OF RADIUS.
3. THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.

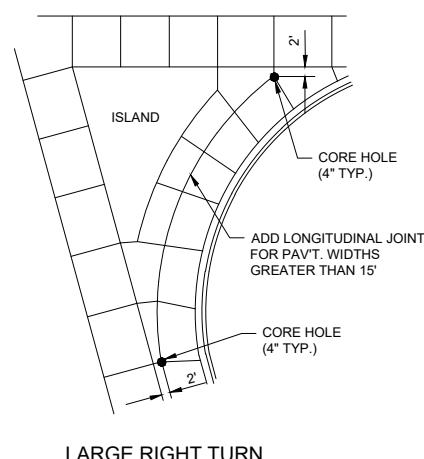


PAVEMENT DEPTH AND JOINT SPACING TABLE

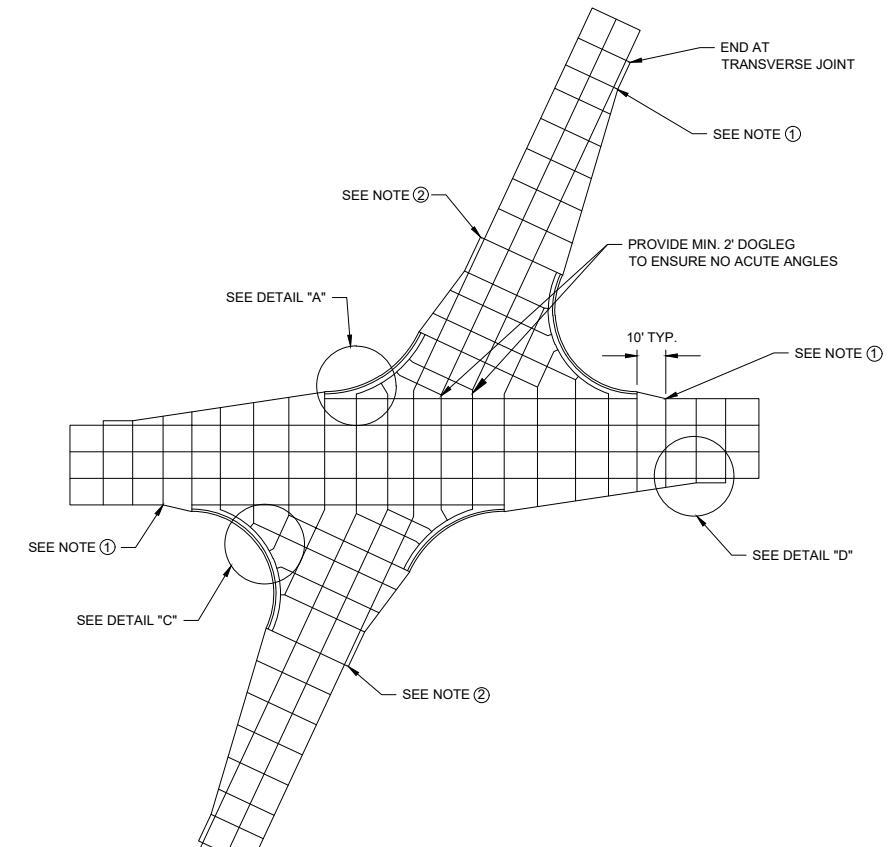
PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6' 6 1/2"	12'
7' 7 1/2"	14'
8" & ABOVE	15'



APPROACH TO MEDIAN

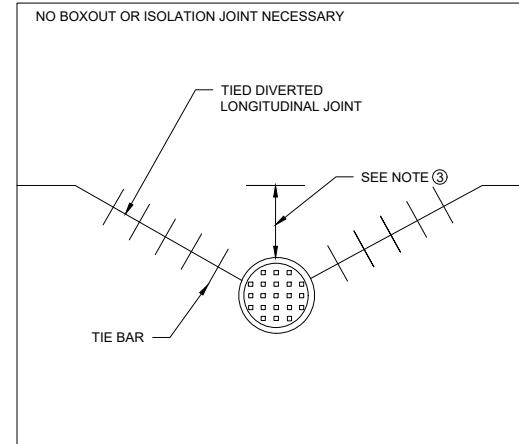


LARGE RIGHT TURN

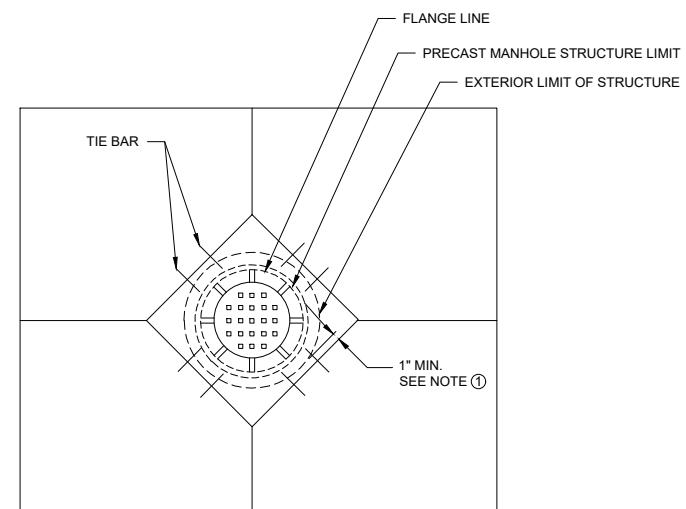


SKEWED INTERSECTION

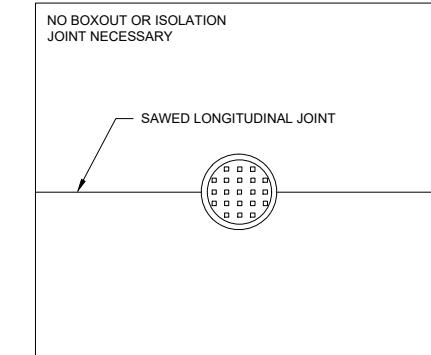
**A** CONCRETE PAVEMENT JOINTING  
 8.4 NOT TO SCALE



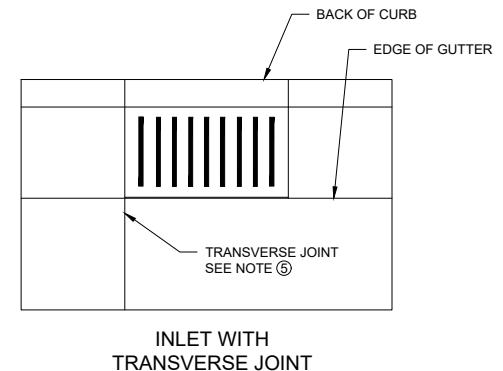
MANHOLE WITH  
DIVERTED  
LONGITUDINAL  
CONTRACTION JOINT



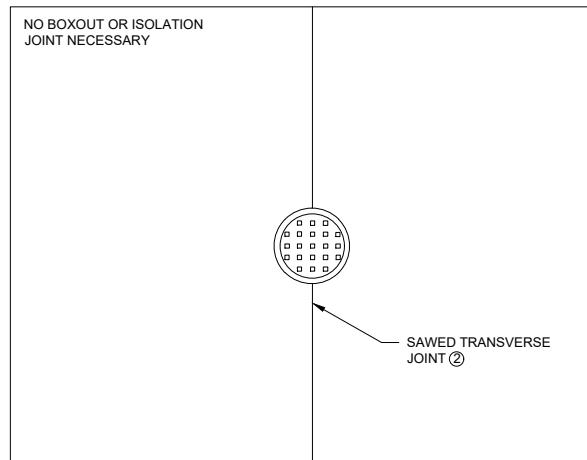
DIAGONAL MANHOLE  
BOXOUT  
FOR CONSTRUCTION  
JOINTS



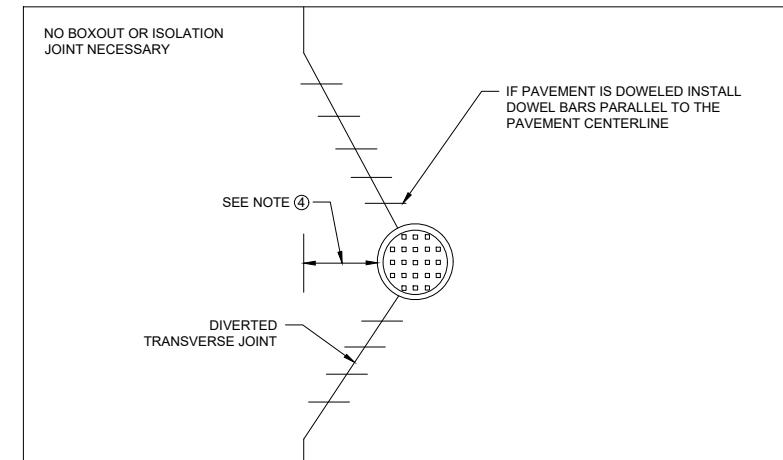
MANHOLE WITH  
LONGITUDINAL JOINT



INLET WITH  
TRANSVERSE JOINT



MANHOLE WITH  
TRANSVERSE JOINT



MANHOLE WITH DIVERTED  
TRANSVERSE CONTRACTION JOINT

#### GENERAL NOTES

1. USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1-FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
2. ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
3. IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
4. IF DISTANCE FROM EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
5. ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

REV	DATE	BY	DESCRIPTION
	03/03/2022		ISSUED FOR BID

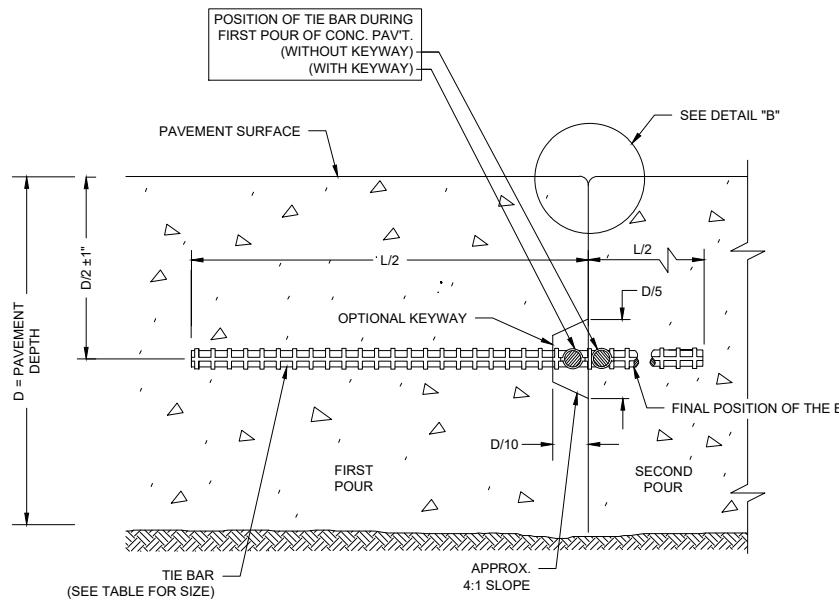
DESIGNED:  
DRAWN: DEM  
CHECKED: REO  
APPROVED: CLM

PROJECT NUMBER  
2022003  
SHEET REFERENCE NO.

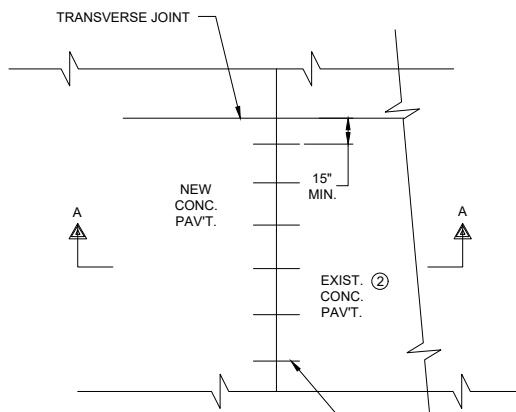




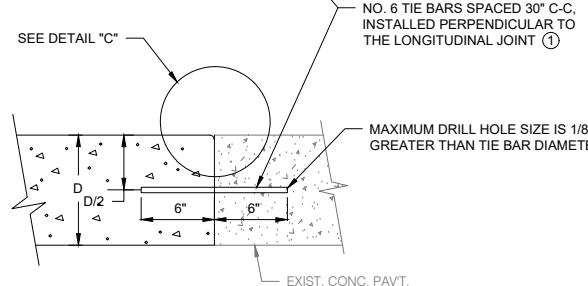
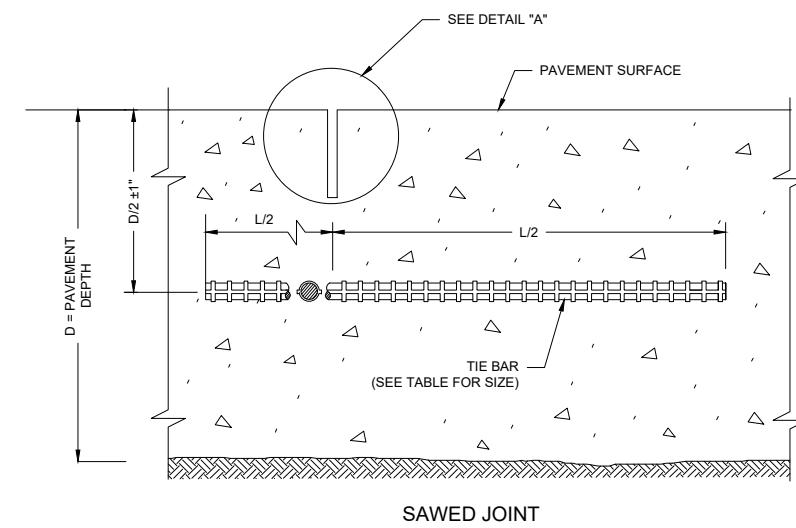




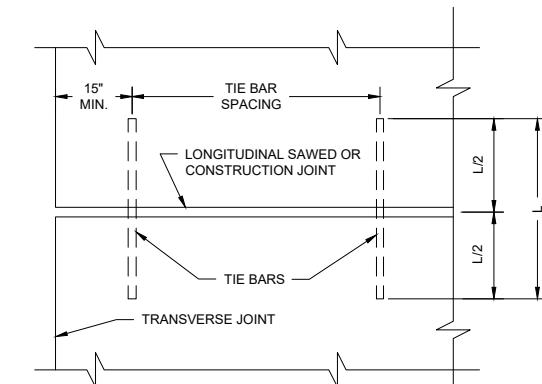
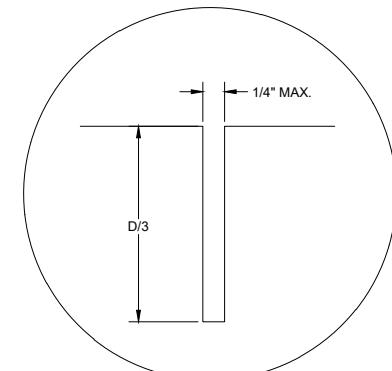
CONSTRUCTION JOINT



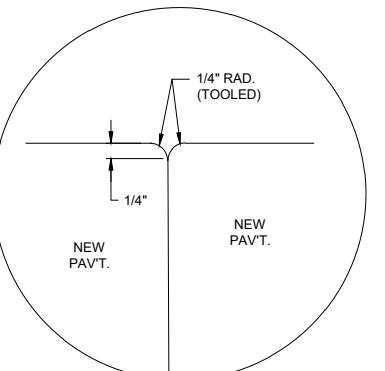
PLAN VIEW

SECTION A - A  
LONGITUDINAL CONSTRUCTION JOINT  
TIE BARS ANCHORED  
INTO EXISTING PAVEMENT

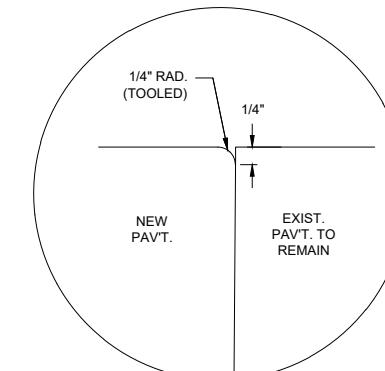
SAWED JOINT

PLAN VIEW  
SHOWING LOCATION OF TIE BARS

DETAIL "A"



DETAIL "B"



DETAIL "C"

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
<10-1/2"	NO. 4	30"	36"
≥10-1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

\* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

\*\* CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

**A** LONGITUDINAL JOINTS AND TIES  
8.9 NOT TO SCALE

## GENERAL NOTES

CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

		BID # 1	BID # 2	BID # 3
		SANITARY MH	EXTERNAL	EXTERNAL CHIMNEY
		4' DIA	CHIMNEY SEAL	SEAL EXTENSION
		V.F.	EACH	EACH
PINE STREET	M-1	11.56	1	
PINE STREET	M-2	10.73	1	
PINE STREET	M-3	12.21	1	
PINE STREET		34.5	3	0
UNDISTRIBUTED		0	0	1
<b>CONTRACT TOTALS</b>		<b>34.5</b>	<b>3</b>	<b>1</b>

8" PVC Sanitary Sewer Main			
	STRUCTURES		
	START	END	LENGTH
PINE ST	M-1	M-2	29.8
PINE ST	M-2	M-3	217.0
UNDISTRIBUTED			3.2
	<b>BID # 5 TOTALS</b>		<b>250.0</b>

12" PVC Sanitary Sewer Main			
	STRUCTURES		
	START	END	LENGTH
PINE ST	M-1	CO-1	5.0
PINE ST	M-2	M-3	5.0
UNDISTRIBUTED			0.0
	<b>BID # 6 TOTALS</b>		<b>10.0</b>

6" PVC Sanitary Sewer Lateral			
	LATERAL		
			LENGTH
PINE ST	111 E. MAIN ST		24.0
PINE ST	119 E. MAIN ST		14.0
PINE ST	517 PINE ST		24.0
UNDISTRIBUTED			3.0
	<b>BID # 4 TOTALS</b>		<b>65.0</b>

	BID # 7
	SANITARY
	ULO
	EACH
UNDISTRIBUTED	1.0
<b>CONTRACT TOTAL</b>	<b>1.0</b>

		BID # 8	BID # 9	BID # 10
		CONNECT	SANITARY	SANITARY
		TO EX.	CLAY	8"x6"
	#	STA.	SANITARY	DAM
PINE ST	CO-1	4+81 LT	1	
PINE ST	CO-2	4+81 RT	1	
111 E. MAIN ST		4+08 RT	1	1
119 E. MAIN ST		4+43 RT	1	1
517 PINE ST		2+37 RT	1	1
EASEMENT			2	3
UNDISTRIBUTED				
<b>CONTRACT TOTALS</b>		<b>2</b>	<b>3</b>	<b>3</b>

		BID # 11
		REMOVE
		EXISTING SAN.
	SHEET #	MH
PINE STREET	2.0	2
PINE STREET		2
UNDISTRIBUTED		
<b>CONTRACT TOTALS</b>		<b>2</b>

		BID # 12	BID # 13
		SAND FILL	ABAN
		10" SAN.	EX. 10"
	STA.	STA.	L.F.
PINE ST	2+80	4+55	175
PINE ST	1+45	2+80	
PINE ST	4+65	4+80	15
EASEMENT		175	150
UNDISTRIBUTED			
<b>CONTRACT TOTALS</b>		<b>175</b>	<b>150</b>

## SANITARY SEWER QTY'S

6" PVC Water Main			
	FITTINGS		
	START	END	LENGTH
PINE ST	R-1	B-1	1.5
PINE ST	B-1	B-2	1.5
PINE ST	B-2	V-2	1.4
PINE ST	V-2	H-2	7.8
PINE ST	T-1	V-1	1.4
PINE ST	V-1	H-1	2.0
UNDISTRIBUTED			0.4
<b>BID # 15</b>			<b>16.0</b>

8" PVC Water Main			
	FITTINGS		
	START	END	LENGTH
PINE ST	CO-5	T-1	1.4
PINE ST	T-1	CO-6	15.9
UNDISTRIBUTED			2.7
	<b>BID # 16</b>		<b>20.0</b>

1" Water Service			
	SERVICE		
	ADDRESS	LENGTH	
PINE ST	517 PINE ST	12.0	
PINE ST	111 E. MAIN ST	8.0	
PINE ST	119 E. MAIN ST	20.0	
UNDISTRIBUTED			
	<b>BID # 17</b>		<b>20.0</b>

	BID # 18	BID # 19	BID # 20
	CONNECT TO	1" WATER	WATER SERVICE
	EX. 8" WM	SERVICE	CLAY DAM
	STA.	EACH	EACH
PINE ST	CO-5	4+28 LT	1
PINE ST	CO-6	4+45 LT	1
PINE ST	CO-8	1+74 LT	1
PINE ST			
PINE ST		1	1
PINE ST		1	1
PINE ST		1	1
PINE ST		3	3
UNDISTRIBUTED			
<b>CONTRACT TOTALS</b>		<b>3</b>	<b>3</b>

	BID # 21
	6"
	VALVE
	STA.
	OFFSET
PINE STREET	V-1
PINE STREET	4+29
PINE STREET	8.6 LT
PINE STREET	1
PINE STREET	V-2
PINE STREET	1+72
PINE STREET	7.2 LT
PINE STREET	1
UNDISTRIBUTED	
<b>CONTRACT TOTALS</b>	<b>2</b>

	BID # 23	BID # 24
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4" PVC LATERAL			
LATERAL			
ADDRESS	STATION	LENGTH	
PINE STREET	119 E. MAIN ST	4+33	21.0
UNDISTRIBUTED		4.0	
BID # 30 TOTALS		25.0	

8" PVC LATERAL			
LATERAL			
ADDRESS	STATION	LENGTH	
PINE STREET	111 E. MAIN ST	3+32	23.0
UNDISTRIBUTED		2.0	
BID # 31 TOTALS		25.0	

12" PVC Storm Sewer Main			
STRUCTURES			
START	END	LENGTH	
PINE STREET	M-14	M-13	18.3
PINE STREET	CO-7	M-13	25.0
UNDISTRIBUTED			1.7
BID # 32 TOTALS		45.0	

21" RCP Storm Sewer Main			
STRUCTURES			
START	END	LENGTH	
PINE STREET	M-10	M-11	7.1
PINE STREET	M-11	M-12	18
PINE STREET	M-12	M-13	122.6
UNDISTRIBUTED			2.3
BID # 33 TOTALS		150.0	

36" RCP Storm Sewer Main			
STRUCTURES			
START	END	LENGTH	
PINE STREET	M-10	CO-3	5
PINE STREET	M-10	CO-4	5
UNDISTRIBUTED			
BID # 34 TOTALS		10.0	

		BID # 35	BID # 36	BID # 37	BID # 38	BID # 39	BID # 40	BID # 41
		STORM MH	STORM INLET	2'x3'	2'x3' CATCH BASIN	ADJUST EX.	REPLACE EX.	REPLACE EX.
		5' DIA	MH 4' DIA	STORM INLET	W/ SUMP	STORM STRUCTURE	MH CASTING	INLET CASTING
		V.F.	EACH	EACH	EACH	EACH	EACH	EACH
PINE STREET	M-10	9						
PINE STREET	M-11			1				
PINE STREET	M-12			7.98				
PINE STREET	M-13			8.32				
PINE STREET	M-14				1			
PINE STREET		9	16.3	1	1			
UNDISTRIBUTED					2	1	1	
<b>CONTRACT TOTALS</b>		<b>9</b>	<b>16.3</b>	<b>1</b>	<b>1</b>	<b>2</b>	<b>1</b>	<b>1</b>

		BID # 42	BID # 43	BID # 44	BID # 45
		CONNECT	CONNECT	CONNECT	STORM SEWER
		TO EX.	TO EX.	TO EX.	LATERAL
		STA.	STORM SEWER	STORM INLET	STORM LATERAL
					CLAY DAM
PINE STREET	CO-3	4+63	1		
PINE STREET	CO-4	4+63	1		
PINE STREET	CO-7	2+98		1	
PINE STREET	111 E. MAIN ST			1	1
PINE STREET	119 E. MAIN ST			1	1
PINE STREET		2	1	2	2
UNDISTRIBUTED					
<b>CONTRACT TOTALS</b>		<b>2</b>	<b>1</b>	<b>2</b>	<b>2</b>

REMOVE EX. 30" OR SMALLER STORM			
STATION			
		START	END
PINE STREET	INLET	TIL0345	40
PINE STREET	TIL0345	TIL1660	108
PINE STREET	TIL1660	TMH0597	15
PINE STREET	TIL1661	TIL1660	24
UNDISTRIBUTED			3
<b>BID # 46 TOTALS</b>		<b>190</b>	

REMOVE EX. INLET			
STRUCTURE	STA.	EACH	
PINE STREET	TIL0345	3+32 LT	1
PINE STREET	TIL1660	4+33 RT	1
PINE STREET	TIL1661	4+44 LT	1
UNDISTRIBUTED			
<b>BID # 47 TOTALS</b>		<b>3</b>	

	BID # 48
	12"
	BULKHEAD
	EACH
	UNDISTRIBUTED
	<b>CONTRACT TOTAL</b>
	<b>1.0</b>

	BID # 49
	STORM
	ULO
	EACH
	UNDISTRIBUTED
	<b>CONTRACT TOTAL</b>
	<b>1.0</b>

	BID # 97	BID # 98	BID # 99	BID # 100
	SILT	12" SEDIMENT	INLET PROTECTION	TRACKOUT
	FENCE	LOG	TYPE D-HR	CONTROL
	SHEET	L.F.	L.F.	EACH
PINE STREET	6.0	105	15	13
PINE STREET		105	15	13
UNDISTRIBUTED	5	20	1	0
<b>CONTRACT TOTALS</b>	<b>110</b>	<b>35</b>	<b>14</b>	<b>1</b>

## STORM SEWER QTY'S

	BID # 51	BID # 52	BID # 53	BID # 54	BID # 55	BID # 56	BID # 57	BID # 58	BID # 59	BID # 60	BID # 61	BID # 62	BID # 63	BID # 64	BID # 65	BID # 66	BID # 67	BID # 68	BID # 69	BID # 70	BID # 71	BID # 72	


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Dept. of Public Works  
108 W. Main Street

Little Chute, WI 54140

## STORM SEWER COMPUTATIONS FOR:

Project Number:

PINE STREET PARKING LOT

Sheet No. 1 of 1  
Computed By: KAL  
Date: 4/27/2022

SEWER LOCATION	DRAINAGE AREA			RAINFALL AND RUNOFF DATA				RUNOFF	SEWER DESIGN COMPUTATIONS								TIME IN SEWER		ELEVATIONS			Cover To Crown (feet)					
	Upstream Manhole	Down stream Manhole	Individual Area (acres)	Sum of Area (acres)	C Individual Impervious coeff.	a	A	C	Individual Ca	Sum of Ca	Rainfall Frequency (years)	Rainfall Intensity (in./hr.)	Total Drainage (cfs)	Length of Sewer (feet)	Manning's Number	Pipe Diameter (inches)	Necessary Drop (feet)	Actual Drop (feet)	Slope (ft./ft.)	Parts Full	Pipe Cap. (cfs)	Velocity Actual (cfs)	Time of Flow in Section (min.)	Total Elapsed Time (min.)	Upstream M.H. Rim Elevation (feet)	Invert Elevations	Upper End (feet)
M-14	M-13	0.39	0.39	0.95	0.37	0.37	10	7.61	2.82	18.29	0.011	12	0.08	0.37	0.02000	0.48	5.95	7.49	0.04	1.04	739.53	736.53	736.16	2.00			
M-13	M-12	0.02	0.58	0.95	0.02	0.55	10	7.59	4.18	122.58	0.013	21	0.09	0.61	0.00500	0.42	11.20	4.33	0.47	1.51	739.55	730.47	729.86	2.39			
M-12	M-11	0.10	0.68	0.95	0.10	0.65	10	7.40	4.78	18.02	0.013	21	0.02	0.09	0.00500	0.46	11.20	4.48	0.07	1.58	737.84	729.86	729.77	6.23			
M-11	M-10	0.08	0.76	0.95	0.08	0.72	10	7.38	5.33	7.11	0.013	21	0.01	0.04	0.00500	0.48	11.20	4.63	0.03	1.61	737.52	729.77	729.73	6.00			
EX INLET	M-13	0.17	0.17	0.95	0.16	0.16	10	7.61	1.23	24.95	0.011	12	0.02	0.87	0.03500	0.27	7.88	7.31	0.06	1.06	739.02	737.37	736.50	0.65			
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